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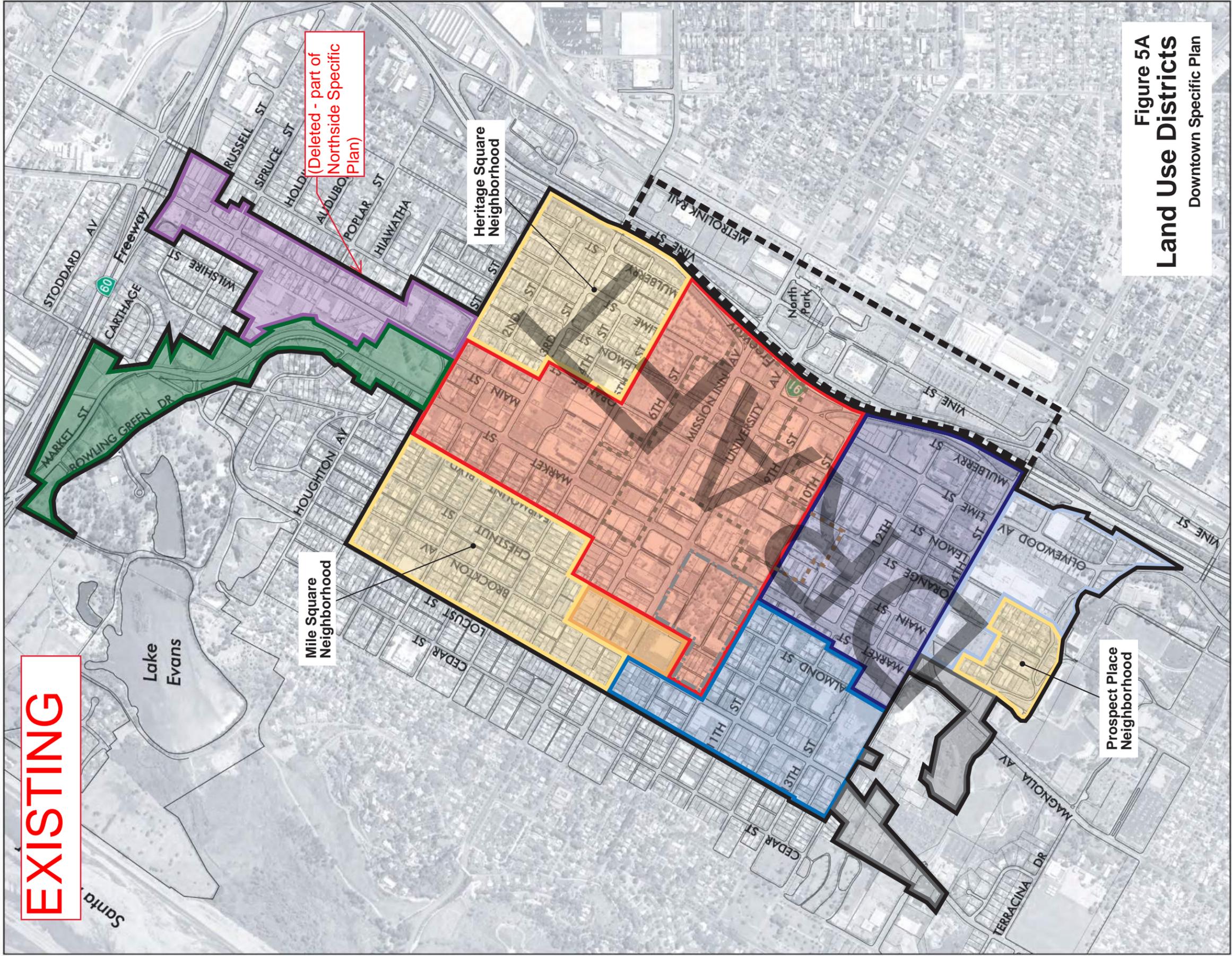


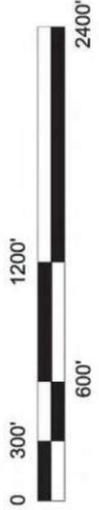
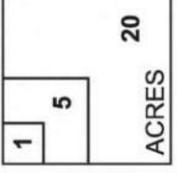
Figure 5A
Land Use Districts
Downtown Specific Plan

LEGEND

LAND USE DISTRICTS

- Raincross
- Justice Center
- Almond Street
- Prospect Place Office
- Health Care
- ~~North Main Street~~
Specialty Services
- Market Street Gateway
- Residential
- Neighborhood Commercial

- Downtown Specific Plan Boundary (Revised May 2014)
- Mission Inn Historic District
(area of special development standards)
- Master Plan Area for the Arts
Riverside School for the Arts
- Riverside Marketplace



PROPOSED

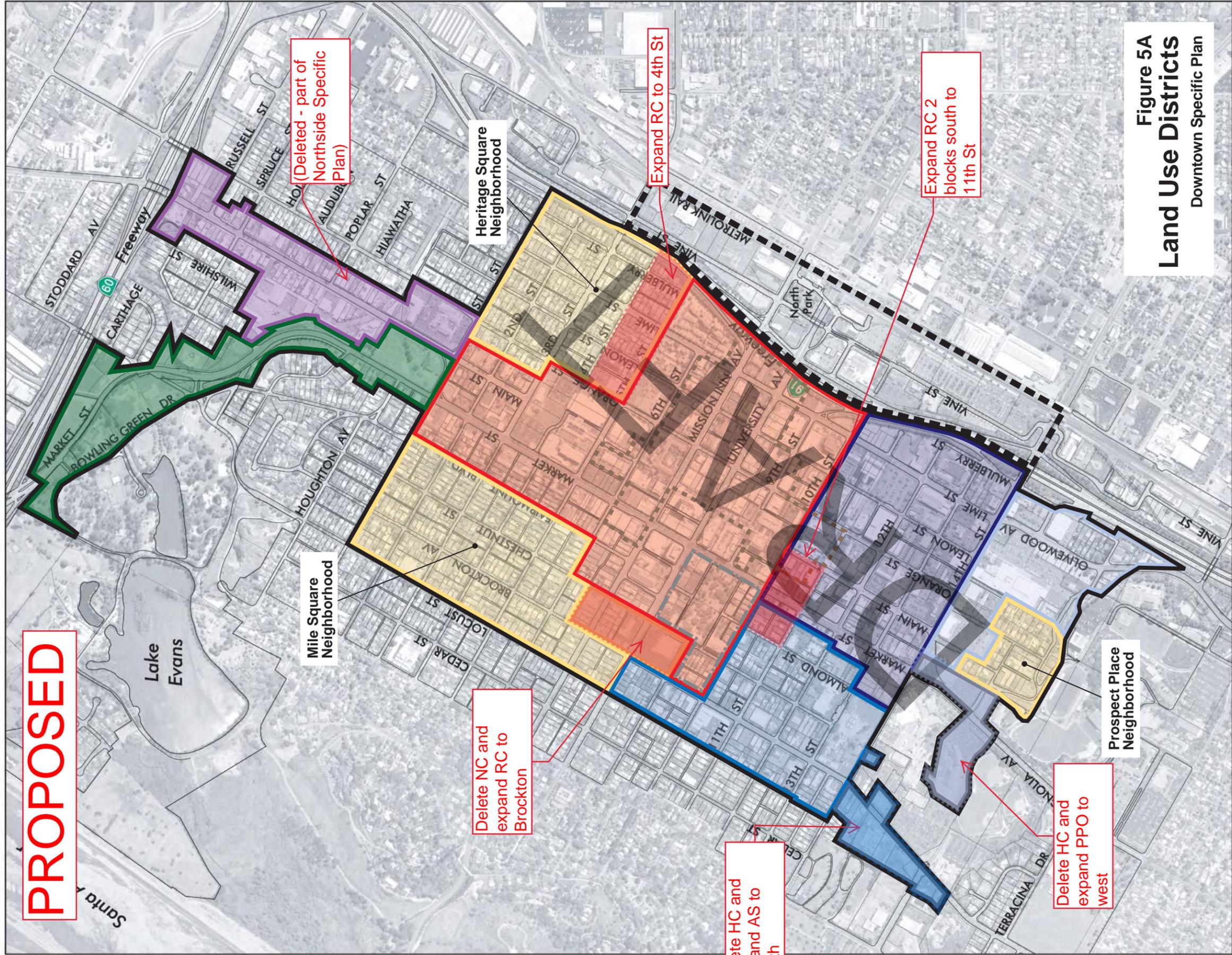
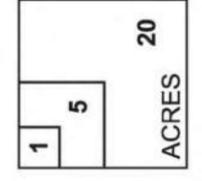


Figure 5A
Land Use Districts
Downtown Specific Plan

LEGEND

- LAND USE DISTRICTS**
- Raincross
 - Justice Center
 - Almond Street
 - Prospect Place Office
 - Health-Care
 - North Main Street Specialty-Services
 - Market Street Gateway
 - Residential
 - Neighborhood-Commercial

- Downtown Specific Plan Boundary (Revised May 2014)
- Mission Inn Historic District (area of special development standards)
- Master Plan Area for the Arts Riverside School for the Arts
- Riverside Marketplace



PROPOSED

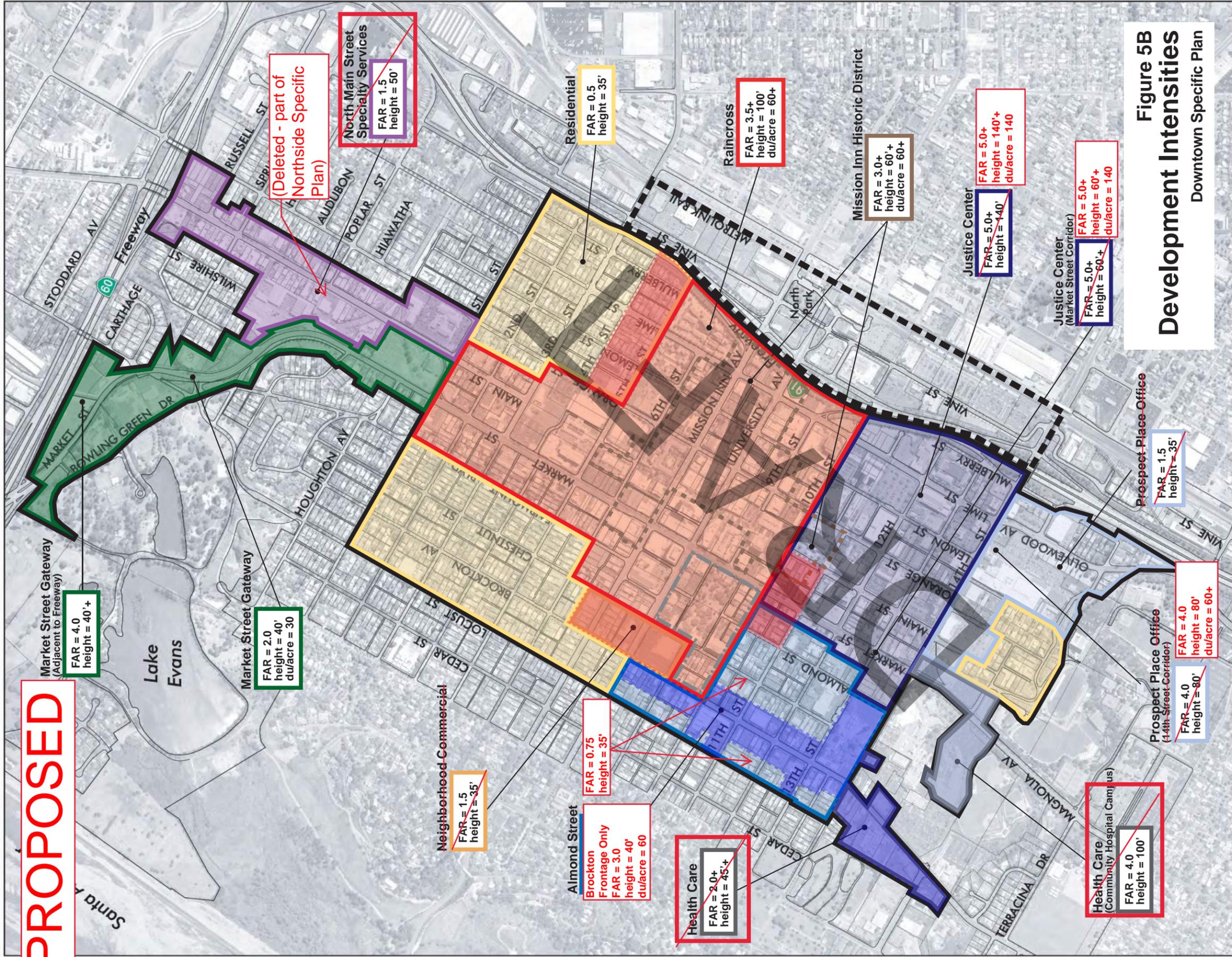
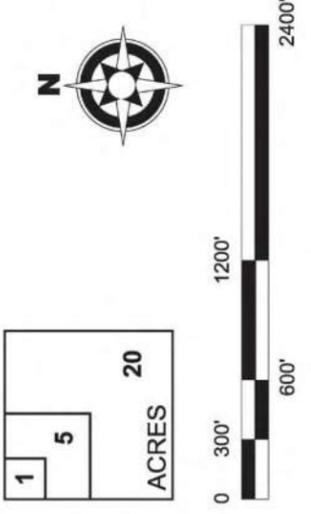


Figure 5B
Development Intensities
Downtown Specific Plan



LEGEND

- LAND USE DISTRICTS**
- Raincross
 - Justice Center
 - Almond Street
 - Prospect Place Office
 - Health Care
 - North Main Street Specialty Services
 - Market Street Gateway
 - Residential
 - Neighborhood Commercial
- Other Features:**
- Downtown Specific Plan Boundary (Revised May 2014)
 - Mission Inn Historic District (area of special development standards)
 - Master Plan Area for the Arts
 - Riverside School for the Arts
 - Riverside Marketplace
- Intensity Indicators:**
- FAR: Maximum Floor Area Ratio Permitted
 - height: Maximum Height Permitted
 - du/acre: Maximum Dwelling Units per Acre Permitted
 - + Intensity or height may be increased with additional approvals

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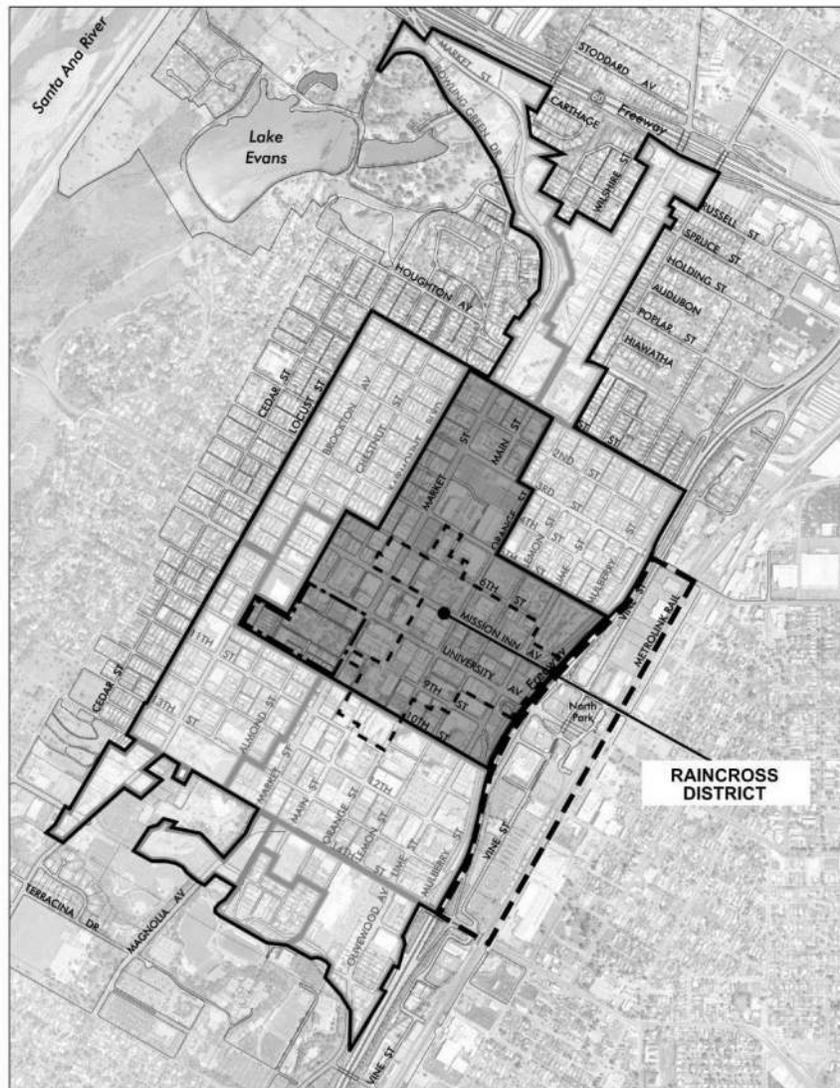
CHAPTER 6

RAINCROSS DISTRICT

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This Chapter defines the land uses, development standards and design standards and guidelines for the Raincross District. This Chapter is organized as follows:

- 6.1 Purpose
- 6.2 Permitted Uses
- 6.3 Conditionally Permitted Uses
- 6.4 Prohibited Uses
- 6.5 Development Standards for the Raincross District
- 6.6 Design Standards and Guidelines for the Raincross District
- 6.7 Additional Standards for Live/Work Units in the Raincross District
- 6.8 Additional Standards for Mixed-Use Development in the Raincross District
- 6.9 Additional Standards for Multiple Family Residential Development in the Raincross District



6.1 PURPOSE

The Raincross District is the cultural, historic, and social center of both Riverside and the region beyond. The quality of Downtown Riverside's historic buildings and the relationship between these buildings creates an historic urban fabric unparalleled in the region. The positive image and economic health of Riverside is strongly influenced by this historic character and the protection of that is an essential part of assuring Riverside's economic health and growth into the future. The intent of this specific plan is to create and encourage opportunities for a variety of commercial, residential and entertainment uses, including retail, office, cultural, residential, and visitor serving uses. The Raincross District encourages a concentration of uses that generate activity during both daytime and evening hours. The permitted retail uses in this District are intended to serve the specialty shopping needs of Downtown residents and employees, as well as citywide residents, regional shoppers, and tourists. The Raincross District encourages these uses to occur as both single use buildings and as mixed-use buildings.

The Raincross District is divided into two sub-areas. The center of the District is occupied by the Mission Inn Historic District, which contains Riverside's most important historic buildings. In this sub-area the development standards have been carefully crafted to maintain a scale of development that is compatible with the well-established historic fabric of the district. Outside of the Mission Inn Historic District, the development standards of the District allow greater intensity, while still assuring compatibility of the adjacent historic district and historic residential areas beyond. The development standards for the Raincross District are designed to create a place of daytime, evening and weekend activity by providing a high activity pedestrian environment with a storefront emphasis at the street level. Within the Raincross District, there are numerous local and national historic landmarks that define the district's character, including the Mission Inn, Fox Theater, Stalder Building, Municipal Museum, Unitarian Church, Congregational Church, Municipal Auditorium, Post Office, Loring Building, and Art Museum. Preservation of such structures, along with careful and compatible design of new development is important in maintaining the District's character and unique sense of identity.

The area between Market Street, Fairmount Boulevard, First Street, and Sixth Street is an area of mixed single family, multiple family, vacant property, and commercial uses. Some of the structures in this area have been identified as having historic significance (Downtown Specific Plan/Mile Square West Survey Project). The transition of uses in this area is, therefore, of particular concern. In accordance with Title 20 of the Municipal Code, any development within this area must give careful attention to the preservation of these historic resources, either by retention on site or by relocation to an appropriate area.

6.2 PERMITTED USES

The following uses are permitted in the Raincross District within 165 feet of Fairmount Boulevard between First and Sixth Streets and within the Heritage Square Historic District:

- a) Home occupations pursuant to the standards established in the Zoning Code.
- b) Live/work units pursuant to the standards set forth in Section 6.7 of this Chapter.
- c) Mixed-use developments consisting of commercial, office and residential uses only and pursuant to the standards set forth in 6.8 of this Chapter.
- d) Multiple-family residential projects pursuant to the standards set forth in Section 6.9 of this Chapter having frontage on Main Street (between Third and Tenth Streets) Mission Inn Avenue, or University Avenue are permitted above the first floor only.
- e) Offices, including but not limited to administrative, general, government and professional.

The following uses are permitted in the remainder of the Raincross District:

- a) Art galleries.
- b) Banking establishments.
- c) Catering businesses.
- d) Education facilities, including vocational schools, with 30,000 square feet or less of cumulative floor area.
- e) Exercise facilities.
- f) Home occupations pursuant to the standards established in the Zoning Code.
- g) Libraries.
- h) Live/work units pursuant to the standards set forth in Section 6.7 of this Chapter.
- i) Medical and dental offices and laboratories.
- j) Mixed-use development pursuant to the standards set forth in 6.8 of this Chapter.
- k) Multiple-family residential dwellings pursuant to the standards set forth in Section 6.9 of this Chapter. Multiple-family residential projects having frontage on Main Street, Mission Inn Avenue, or University Avenue are permitted above the first floor only.
- l) Museums.
- m) Offices, including but not limited to administrative, general, government and professional, except on Main Street where they may only be located off-street or behind retail or restaurant uses.
- n) Parks and open spaces.
- o) Personal service establishments, such as, but not limited to, barber or beauty shops, tailors, shoe repair, etc.
- p) Pharmacies.
- q) Restaurants, including outdoor dining, pursuant to the standards established in the Zoning Code for outdoor dining.
- r) Retail sales.
- s) Temporary uses, pursuant to the standards established in the Zoning Code and consistent with the purposes of this District.
- t) Wireless communication facilities, pursuant to the Zoning Code.
- u) Accessory uses, incidental and subordinate to the principal permitted use.

u) Other uses, not listed above, which are determined by the ~~Planning Director~~ Community & Economic Development Director, or his/her designee, to be similar to those listed above.

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6.3 CONDITIONALLY PERMITTED USES

The following uses are permitted with a Minor Conditional Use Permit in the Raincross District within 165 feet of Fairmount Boulevard between First and Sixth Streets and within the Heritage Square Historic District:

- a) Bed and breakfast inns.
- b) Minor expansions to nonconforming uses with a previously approved Conditional Use Permit.
- c) Vapor recovery systems, pursuant to the Zoning Code.
- d) Wireless communication facilities, pursuant to the Zoning Code.

The following uses are permitted in the remainder of the Raincross District with a Minor Conditional Use Permit:

- a) Arcades.
- b) Assemblies of People – Entertainment, not listed below.
- c) Assemblies of People – Non-Entertainment, not listed below.
- d) Bed and breakfast inns.
- e) Brewpub, Breweries, Microbreweries, and Wineries (with off-sale retail and/or on-site tasting), Brew-on-premises (with tasting and/or retail sales of prepared beers) pursuant to the standards established in the Zoning Code.
- f) Farmer’s market.
- g) Florist with incidental wine sales.
- h) Indoor multi-tenant sales with 25,000 square feet or less.
- i) Minor expansions to nonconforming uses with a previously approved Conditional Use Permit.
- j) On-sale of alcoholic beverages (drinking establishments such as bars, nightclubs, pubs or taverns, whose primary business is the sale of alcohol), pursuant to the standards established in the Zoning Code.
- k) Outdoor food preparation.
- l) Pool hall.
- m) Vapor recovery systems, pursuant to the Zoning Code.
- n) Wireless communication facilities, pursuant to the Zoning Code.

The following uses are permitted with a Conditional Use Permit in the Raincross District within 165 feet of Fairmount Boulevard between First and Sixth Streets and within the Heritage Square Historic District:

- ~~a) Commercial coach as a temporary office.~~
- ~~b)a) Expansion of nonconforming uses.~~
- ~~e)b) Parking structures and facilities with ground floor office, residential, or live/work uses.~~
- ~~d)c) Wireless communication facilities, pursuant to the Zoning Code.~~

The following uses are permitted in the remainder of the Raincross District with a Conditional Use Permit:

- a) Auditorium.
- b) Child day care centers.
- c) Cinemas.

- ~~d) Commercial coach as a temporary office.~~
- e)d) Convention and conference facilities.
- f)e) Department stores.
- g)f) Educational facilities, including vocational schools, with 30,000 square feet or more of cumulative floor area.
- h)g) Expansion of nonconforming uses.
- i)h) Hotels.
- j)i) Indoor multi-tenant sales with more than 25,000 square feet.
- ~~k) Liquor stores, pursuant to the standards established in the Zoning Code.~~
- j) Off-sale of alcoholic beverages (liquor stores, markets, etc.), pursuant to the standards established in the Zoning Code.
- ~~m)k) Parking structures and facilities with ground floor retail and/or office uses.~~
- n)l) Performing arts facilities.
- o)m) Public uses not permitted as a matter of right.
- p)n) Public utilities or installations.
- o) Recreational facilities.
- ~~q)p) Student housing.~~
- r)q) Transit center.
- s)r) Wedding chapels.
- s) Wireless communication facilities, pursuant to the Zoning Code.

6.4 PROHIBITED USES

The following uses are prohibited in the Raincross District:

- a) Car wash.
- b) Drive-thru establishments.
- c) Motels.
- d) Service stations (gas stations and minor repairs).
- e) Vehicle repair.
- f) Vehicle sales and vehicle parts sales.

6.5 DEVELOPMENT STANDARDS FOR THE RAINCROSS DISTRICT

All property in the Raincross District shall be developed in accordance with the following standards. To ensure compatible development with the historic buildings in the Mission Inn Historic District, the maximum allowable height and maximum allowable density in this area is lower than for development in the remainder of the Raincross District. In addition, to protect the adjacent historic residential neighborhood, no vehicular access from or widening of Fairmount Boulevard between First and Sixth Streets should be permitted.

6.5.1 Maximum Floor Area Ratio

- A. Outside the Mission Inn and Heritage Square Historic Districts:

The maximum floor area ratio (FAR) in the Raincross District, outside of the Mission Inn and

Heritage Square Historic Districts, shall be 3.5, except an increased FAR up to, but not exceeding, 4.5 may be permitted for the site amenity features listed in the following table. Amenities may be combined for increased FAR, except that in no case shall the floor area ratio exceed 4.5.

Amenity	Bonus
<p>Landscaped Courtyard or Atrium. A continuous area which is open and accessible to the public, has a minimum area of 600 continuous square feet with a minimum dimension of 20 feet. Such courtyard or atrium shall be landscaped with greenery, statuary, water features, seating, or combination of the four.</p>	<p>Floor area increased four times the square footage contained within the courtyard.</p>
<p>Arcade or Pergola. A covered pedestrian passageway that connects two public streets; a public street and parking area or mall; or a public street and another arcade or pergola. The arcade or pergola shall be unobstructed, have a minimum width of 15 feet and a minimum vertical clearance of 12 feet and shall be situated at street grade. Arcades and Pergola located within the public right-of-way shall require an encroachment permit from the Public Works Department. Where an arcade or pergola is located within the public right-of-way, the minimum width may be reduced to conform with the width of the right-of-way.</p>	<p>Floor area increased three times the square footage contained within the arcade or pergola.</p>
<p>Other Amenities. Additional floor area ratio may be permitted in conjunction with modified amenities or amenities not listed above with the granting of a Conditional Use Permit, provided the proposed amenities specifically support the purpose and intent of the Raincross District and are compatible with surrounding development and design. Specific floor area bonus shall be negotiated and the amenities must be acceptable to the City.</p>	<p>Floor area bonus negotiated in conjunction with the amenities provided and subject to the granting of a Conditional Use Permit.</p>

B. Within the Mission Inn and Heritage Square Historic Districts:

The maximum floor area ratio for parcels within the Mission Inn and Heritage Square Historic Districts shall be 3.0, except for the following:

- (1) Additional floor area ratio may be permitted for certain site amenity features set forth in the above table. Amenities may be combined for increased FAR, except that in no case shall the floor area ratio exceed 4.0.
- (2) In addition to the above amenity provision, floor area ratio within the Mission Inn Historic District may be increased up to 4.5 with the approval of a Conditional Use Permit, provided the proposed use specifically supports the purpose and intent of the Raincross District and is compatible with surrounding development and design.

6.5.2 Maximum Unit Density

The maximum dwelling unit density shall be 60 units per acre. The maximum unit density may be increased with the approval of a Conditional Use Permit.

6.5.3 Maximum Height

A. Outside the Mission Inn Historic District:

The maximum building height in the Raincross District, outside the Mission Inn Historic District, shall be 100 feet, except for the following:

- (1) The maximum building height shall not exceed 50 feet within 100 feet of the Residential District or a residential zone outside the specific plan boundaries. For purposes of this requirement, where the district boundary line runs down the middle of a street or alley or the residentially zoned property is across a street or alley, the 100 feet shall be completely contained on the property proposed for development, not measured from the middle of the street.
- (2) For parcels outside the above referenced 100 feet distance from the Residential District or residential zone, building heights may exceed 100 feet with the granting of a Conditional Use Permit, provided that the maximum FAR is not exceeded.

B. Within the Mission Inn Historic District:

The maximum building height in the Raincross District, within the Mission Inn Historic District, shall be 100 feet, provided that anything over 60 feet requires the approval of a Conditional Use Permit and must specifically support the purpose and intent of the Raincross District and be compatible with surrounding development and design.

C. Within the Heritage Square Historic District:

The maximum building height in the Raincross District, within the Heritage Square Historic District, shall be 50 feet and three stories.

6.5.4 Minimum Lot Size

The minimum lot size for new parcels shall be 10,000 square feet.

6.5.5 Front Yard Setback

There shall be no front yard setback; buildings shall be contiguous with the front parcel line (0-foot setback). The front yard setback shall also apply to side and rear yards adjacent to a public street. The following exceptions apply to front yard setbacks:

- (1) A portion of the front building elevation, not to exceed fifty percent of the length of the building frontage, may be setback up to 20 feet to allow for outdoor use, such as outdoor dining, display, public art, entry forecourts, or other amenity appropriate to an urban setback.
- (2) For parcels that have frontage on Mission Inn Avenue between the 91 Freeway and Main Street, the minimum setback shall be 15 feet. The front yard setback should incorporate a combination of “soft” features, such as landscaping, water, etc. and “hard” features, such as pavers, ironwork fencing, etc. No parking is permitted in the front yard setback.
- (3) Parcels with frontage on Fairmount Boulevard between First and Sixth Streets, shall have a minimum setback along Fairmount Boulevard of 15 feet. This setback should incorporate a

combination of “soft” features, such as landscaping, water, etc. and “hard” features, such as pavers, ironwork fencing, etc. No parking is permitted in this setback.

6.5.6 Rear Yard Setback

No minimum rear yard setback is required, except for the following:

- (1) Where the rear parcel line abuts the Residential District or a residential zone outside the specific plan boundaries, the minimum rear yard setback shall be 15 feet if there is a public alley (distance from building to rear property line or alley easement), and 25 feet where there is no public alley.
- (2) When the project contains a residential component (i.e., multiple family residential use, mixed-use or live/work unit), the ~~building shall be setback a minimum~~ rear yard setback shall be of 15-5 feet from the rear parcel line if there is a public alley, and 15 feet where there is no public alley. This standard shall not apply to adaptive reuse, or conversion of existing buildings into a residential use.
- (3) Where both of the above situations apply, the greater setback standard shall apply.

6.5.7 Interior Side Yard Setback

No minimum interior side yard setback is required, except for the following:

- (1) When the project contains a residential component (i.e., multiple family residential use, mixed-use or live/work unit), the portion of the building containing the residential use shall be setback a minimum of 15-5 feet from the interior side parcel line. This standard shall not apply to adaptive reuse, or conversion of existing buildings into a residential use.

~~6.5.8 — Master Plan for the Riverside School for the Arts~~

~~The proposed Riverside School for the Arts shall be developed as part of a Master Plan approved by the City Council and shall be subject to the development standards and requirements therein.~~

~~6.5.9~~6.5.8 **Multiple-family Residential Projects fronting Main Street, Mission Inn Avenue and University Avenue**

Multiple-family residential projects in the Raincross District that have frontage on Main Street (between Third and Tenth Streets), University Avenue or Mission Inn Avenue must be located above the ground floor or in ground floor rear areas of buildings and shall be combined with a ground floor pedestrian-oriented ~~retail or restaurant~~commercial use, as permitted in this District, to create an active retail edge on these streets. Such projects are subject to standards set forth in Section 6.8 for mixed-use development.

~~6.5.10~~6.5.9 **Parking**

Refer to Chapter 16 for off-street parking requirements and standards.

6.6 DESIGN STANDARDS AND GUIDELINES FOR THE RAINCROSS DISTRICT

6.6.1 District Character Defining Statement

The Raincross District is the cultural, entertainment, and retail center of Riverside and the region beyond. Its significant, signature buildings include the Fox Theater, Stalder Building, Mission Inn, Municipal Museum, Unitarian Church, Congregational Church, Municipal Auditorium, Post Office, Loring Building, and Art Museum. Historic and cultural resource sensitivity are the key concepts in this district. Buildings that contribute to the historic character of this district should be preserved or restored to an authentic historic design. New construction should be in scale and architecturally harmonious with nearby historic buildings. The above listed signature buildings should be used for inspiration regarding design, form, detailing and site layout.

The design standards and guidelines for the Raincross District are intended to enhance both these signature buildings and their setting which together contribute to the character of a cohesive downtown. In addition, the design standards and guidelines for the Raincross District are intended to create a vibrant, pedestrian friendly downtown by encouraging pedestrian orientation to the storefronts, human scaled spaces, and pedestrian amenities.



Fox Theater, 3801 Mission Inn Avenue



Loring Building, 3673 Main Street



Municipal Museum, 3720 Orange Street



Mission Inn, 3645 Mission Inn Avenue



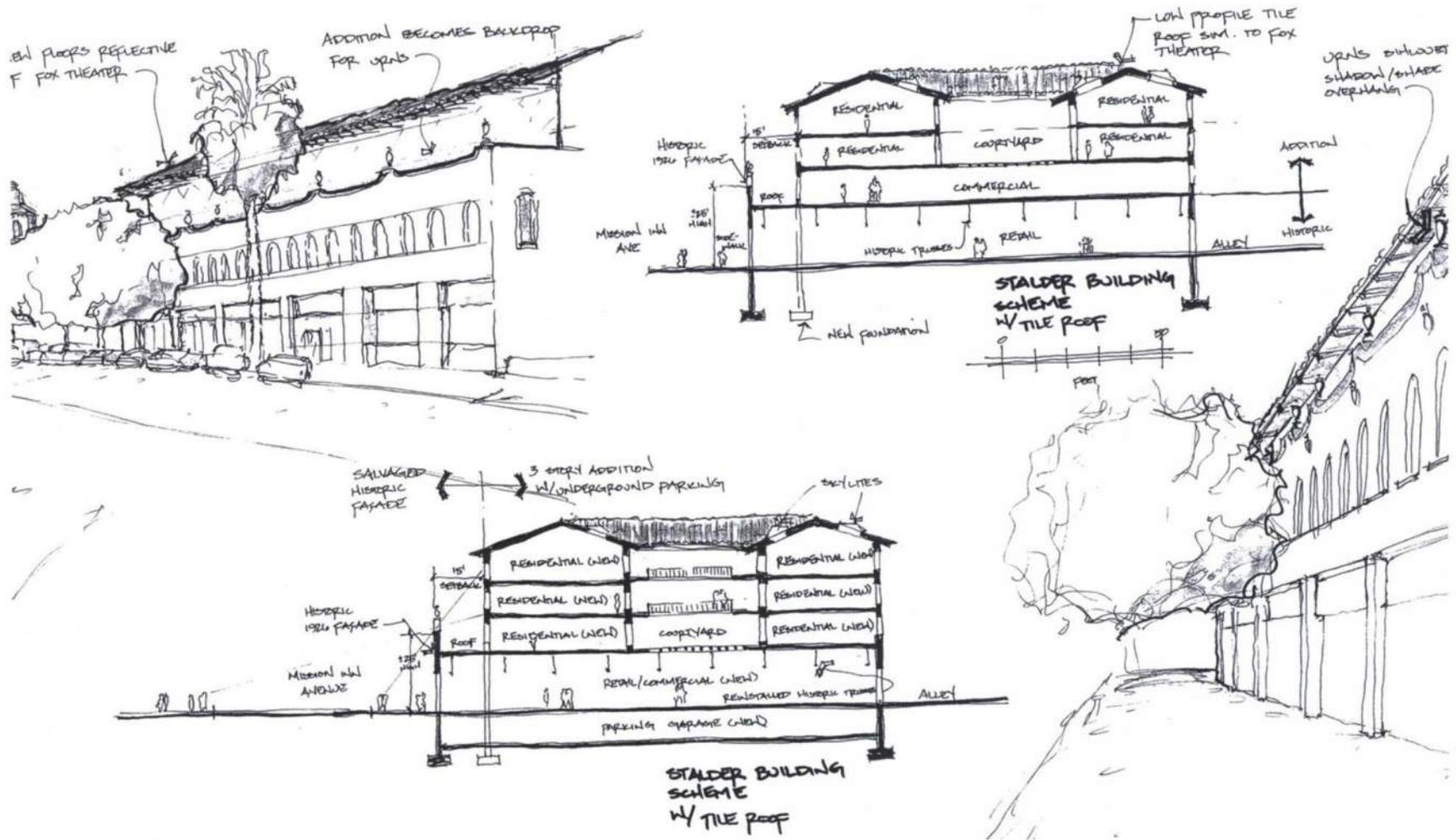
A good example of a preservation sensitive approach to fulfilling the needs for office and commercial lease space in the District’s Mission Inn Historic District can be found in the Stalder Building and Imperial Hardware Building Historic Resources Survey, by architect Wayne Donaldson, completed in January 2001. Mr. Donaldson was hired by the City to investigate development opportunities on the sites currently occupied by the Stalder Building (situated on the east corner of Mission Inn Avenue and Market Street) and the Imperial Hardware Building (situated on the east side of the Downtown Mall, between Mission Inn and University Avenues). A developer interested in creating a “Lifestyle Center” in this area suggested these buildings be removed and replaced with new structures.

The Donaldson report investigates the historic significance of the buildings, their structural integrity, and the potential for a variety of development options. The report demonstrates the pros and cons of everything from full preservation to adaptive reuse options that would preserve various significant historic aspects of the buildings. It also documents the presence of historic structural members hidden inside the Stalder Building and an intact Art Deco façade covered by a 1960’s era metal false front on the Imperial Hardware Building. The report shows how both the Stalder and the Imperial Hardware Buildings could be put to greater economic use with additions that would allow the original historic fabric of the buildings to be preserved and restored. These ideas allow both sites to be developed to the maximum floor area ration (FAR) permitted by the Specific Plan, while preserving the essence of the historic buildings. As development opportunities are explored that affect older buildings in the Raincross District, consideration of various preservation alternatives should be undertaken in the fashion of the Donaldson report.



Stalder Building





Stalder Building - Three additional levels with tile roofs
 Source: Stalder Building and Imperial Hardware Building, Historic Resources Survey, Wayne Donaldson, January 25, 2001



*Imperial Hardware - Westbrook's Hardware Store, ca. 1935. Postcard Courtesy of Riverside Municipal Museum
Source: Stalder Building and Imperial Hardware Building, Historic Resources Survey, Wayne Donaldson, January 25, 2001*



Imperial Hardware - Two additional office levels

Source: Stalder Building and Imperial Hardware Building, Historic Resources Survey, Wayne Donaldson, January 25, 2001

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6.6.2 Site Planning

Building Orientation

- (1) Buildings should have a strong street presence, with public entrances and activity areas oriented toward the street.
- (2) There is a pattern of first floors elevated above sidewalk grade along Mission Inn Avenue between the 91 Freeway and Orange Street. This pattern should be maintained. Building entries should be raised up to three feet from the adjoining street level.

Setbacks

- (1) Commercial buildings should generally have a direct interface with public sidewalks with no intervening setback.
- (2) Buildings along Mission Inn Avenue between the 91 Freeway and Orange Street and between Fairmount Boulevard and Chestnut Street should be setback a distance that is compatible with other nearby buildings.
- (3) These setback areas should generally include a combination of “soft” features (landscaping, water, etc.) and “hard” features (pavers, steps, patios, arcades, porches, etc.). For parcels with frontage on Mission Inn Avenue; architectural elements such as stairs or steps, and urban amenities such as benches, water fountains, and public art are encouraged.

Vehicular Access and Parking

- (1) Parking spaces should not have direct frontage on a street. Access points should be limited to existing alleys wherever possible.
- (2) When on-site parking is provided, parking should be consolidated on the site. In no case, should the building be surrounded on all sides with parking.
- (3) No vehicular access to or from Fairmount Boulevard should be permitted between First and Sixth Streets within this District.

Pedestrian Access

- (1) Primary access to buildings should be from the street or pedestrian walkways, not parking areas.
- (2) Walkways should be provided to link parking areas with the street wherever feasible.

Interface between Non-residential and Residential Uses

In several portions of the Specific Plan area, non-residential uses abut residential uses. This condition can specially be observed in the Raincross District, along Fairmount Boulevard between First and Sixth Streets. Issues of privacy, safety, and noise are addressed in these following standards:

- (1) To provide privacy for adjacent residential properties, taller elements of the building should be set away from those properties. In addition, at residential edges, commercial buildings should maintain low profiles and building heights should be stepped down to the height of adjacent residential zones, utilizing architectural elements such as gables or hip roofs to reduce building mass.

- (2) When there is no intervening alley or street, appropriate landscape screening shall be provided at the shared property line. Excepting trees, this screening shall not be less than six feet or exceed eight feet in height.
- (3) Eighty percent of the vertical plane at the property line to a height of six feet shall be opaque.
- (4) Screening may consist of one (or more) of the following:
 - “Vertical” trees closely spaced
 - “Green” (vine-covered) solid or fenced walls
 - Hedges (minimum height of six feet)
- (5) The criteria for selecting plant materials, as established in section 15.3.6 shall be followed.
- (6) Noise or odor generating activities in general, and loading areas, trash and storage areas, and rooftop equipment in particular, should be located as far as possible from adjacent residential uses and shall not be located next to residential properties without fully mitigating their negative effects.
- (7) Non-residential buildings should be sited so as to avoid significant shading of adjacent residences and compromising residents’ privacy.
- (8) Windows in non-residential buildings should be oriented to avoid a direct line of sight into adjacent residential buildings or property.
- (9) Whenever adjacent residential and commercial uses can mutually benefit from connection rather than separation, appropriate connective elements such as walkways, common landscaped areas, building orientation, gates, and/or unfenced property lines should be employed.

Site Furniture

- (1) Pedestrian amenities are encouraged, including benches, landscaped gathering areas, trash receptacles, etc. Design of improvements should be traditional and related to the signature buildings. Particular attention should be paid to creating shade in the Raincross District as well as all the other districts.

Courtyards and Passages

- (1) When placed in an appropriate location, between two elements of a building or buildings, a courtyard can provide a visually relaxing pedestrian environment, or a secluded retreat from noise and traffic. Courtyards play an important role in providing spaces for solace and respite in Downtown. Courtyards and pedestrian passages are encouraged to facilitate pedestrian circulation and to provide resting and gathering places. Courtyards should be prominently placed on the site to be seen from the street and to enhance the public environment. They should be placed to terminate vistas, to anchor street corners and along street edges.
- (2) All courtyards should be provided with amenities such as shade trees, seating areas, water fountains, accent planting, and public art. If space permits, elements such as gazebos, arcades, or pergolas should be incorporated in the courtyard design.
- (3) Design of improvements should be traditional and related to the signature buildings.

6.6.3 Architecture

Style

- (1) Existing buildings should be restored/maintained in a historic style that reflects the actual, historic appearance of the building at its period of historic significance.
- (2) The historic fabric in Downtown Riverside is interspersed with “contextual” buildings - buildings that are not historic but contribute to the district character as one traverses the district. Similarly, new buildings should not necessarily be stylistically “historic”, but should be compatible with their historic neighbors in terms of massing, modulation, height, and setbacks. New buildings should be contemporary interpretations using the signature buildings as a source of design inspiration.

Scale

- (1) Buildings and improvements should be at a pedestrian scale. To maintain a sense of pedestrian scale, larger buildings should be broken into storefront bays about 25 feet wide.
- (2) The size and mass of a new building should blend with the surrounding district.

Detailing

- (1) Detailing of existing buildings should be a restoration or replication of historic detailing during the building’s period of historic significance.

Roof Design

- (1) Roof design should reflect/complement significant buildings in the area.

Colors and Materials

- (1) Muted earthtones and traditional materials should prevail, with brighter colors limited to trim areas. The Mission Inn is a good example of this type of treatment.

6.6.4 Landscaping

Plant Types

- (1) Landscaping should be compatible with historic plantings and consist of types suitable for the climate and the exposure in which they are to be planted.

Scale

- (1) Pedestrian scale plantings should prevail, with larger plantings used as accents.

Relationship to Development

- (1) Plantings should be used to complement and accent the architecture. They should not be of such a scale and density as to obscure or overwhelm the architecture.

Hardscape

- (1) Paved areas should make significant use of traditional concrete scoring and pavers.

6.6.5 Signs

Style

- (1) Signs should be low-key and complementary to the architecture. Eating and entertainment uses may make greater use of color and innovative design. Historic forms and types should predominate.

Scale

- (1) Signing should be an accent; the overall effect should be low key and proportional to the building.

Type

- (1) A variety of forms is acceptable, including painted wood, painted metal, signs painted directly on the building, and traditional neon. Interior illuminated signs, plastic, canister signs, channel letter signs, and other more modern forms of signs are generally not appropriate.

6.6.6 Additional Design Standards and Guidelines

Refer to Chapter 15 for general design standards and guidelines that apply to the entire Downtown Specific Plan area.

6.7 ADDITIONAL STANDARDS FOR LIVE/WORK UNITS IN THE RAINCROSS DISTRICT

6.7.1 Applicability

The provisions of this section apply to live/work units, as defined in Chapter 18: Definitions. These standards are in addition to the development standards set forth in Section 6.5 and the applicable design guidelines. Refer to Chapter 16 for parking standards and Chapter 17 for sign standards. The type of work permitted in a live/work unit shall be consistent with, or similar in nature to, the permitted uses in the Raincross District, as defined in Section 6.2, and home occupations as listed in the Zoning Code.

6.7.2 Purpose

The intent of this section is to provide for and make feasible the reuse of existing commercial or residential structures to accommodate live/work opportunities, as well as provide opportunities for the new development of buildings specifically designed and constructed to provide live/work units. Live/work units may also occupy a portion of a building designed for mixed-use development.

6.7.3 Floor Area Requirements

The minimum floor area of a live/work unit shall be 750 square feet. For live/work units that have frontage on Mission Inn Avenue, Main Street or University Avenue, the first 25 feet of floor area depth at the street-level frontage shall be devoted to pedestrian-oriented commercial retail activity. Live/work units with frontage on public streets in all other areas of the Raincross District, or on the upper levels of a structure shall not be subject to this requirement.

6.7.4 Access to Units

Access to individual units shall be from common access areas, corridors, or hallways.

6.7.5 Internal Layout

All living space within the live/work unit shall be contiguous with and an integral part of the working space, with direct internal access between the two areas.

6.7.6 Street Frontage Treatment

Each live/work quarters fronting Mission Inn Avenue, Main Street or University Avenue shall have a pedestrian-oriented frontage that publicly displays the interior of the nonresidential areas of the structure. Live/work units with frontage on public streets in all other areas of the Raincross District, and on the upper levels of a structure shall not be subject to this requirement.

6.7.7 Occupancy and Employees

At least one of the full-time workers of the live/work unit shall reside in the unit. The residential area shall not be rented separately from the working space. The business activity occupying the live/work unit may utilize employees in addition to residents as necessary.

6.7.8 Retail Sales

Retail space may be integrated with working space.

6.7.9 Business License Tax Certificate

A business license shall be obtained in compliance with the Municipal Code for business activities conducted within the live/work unit.

6.8 ADDITIONAL STANDARDS FOR MIXED-USE DEVELOPMENT IN THE RAINCROSS DISTRICT

6.8.1 Applicability

The provisions of this section apply to mixed-use development, as defined in Chapter 18: Definitions, in the Raincross District. These standards are in addition to the development standards set forth in Section 6.5 and the applicable design guidelines. Refer to Chapter 16 for parking standards and Chapter 17 for sign standards.

6.8.2 Purpose

The intent of this section is to strengthen the interaction between residential, commercial and employment uses so as to facilitate a more efficient use of transportation systems, to encourage the conservation of land resources and create a vital urban area that is a place of daytime, evening and weekend activity.

6.8.3 Land Use Requirements for Mixed-use Development

Mixed-use development integrates compatible office or commercial uses with residential uses within the same building or structure. Mixed-use development in the Raincross District should generally promote retail uses at the street level, and shall have the following use requirements:

1. Ground Floor or Street Level
 - (a) Retail uses - The ground floor or street level shall be devoted to pedestrian-oriented retail, restaurant, or similar type of use, except within 165 feet of Fairmount Boulevard between First and Sixth Street and within the Heritage Square Historic District where retail uses are prohibited.
 - (b) Office uses - General and professional office uses shall be allowed on the ground floor, except on Main Street where they may only be located off-street or behind retail or restaurant uses to create an active retail edge.
 - (c) Residential or lodging uses - Mixed-use projects that have frontage on Main Street between Third and Tenth Streets, Mission Inn Avenue, or University Avenue may have residential or lodging uses on the ground floor only when located off-street or behind retail uses. Residential or lodging uses shall be permitted on the ground floor for all other areas of the Raincross District. A common entrance to the residential portion of the mixed-use project may be located adjacent to the non-residential front, ground floor use.
 - (d) Live/work uses - Live/work units shall be permitted on the ground floor, subject to the standards for live/work units set forth in Section 6.7. For mixed-use projects that have frontage on Main Street, Mission Inn Avenue or University Avenue, the first 25 feet of floor area depth at the street level frontage shall be devoted to pedestrian-oriented commercial retail activity. Live/work units on the ground floor of all other mixed-use projects shall not be subject to this requirement.

2. Upper Levels

- (a) The upper levels may contain retail (except within 165 feet of Fairmount Boulevard between First and Sixth Streets and within the Heritage Square Historic District where commercial uses are prohibited), office or lodging uses, however, at least one floor of the upper levels must be dedicated to residential or live/work uses.

~~6.8.4~~ Lot Area and Width

- ~~1. The minimum lot area for any new mixed-use development shall be 10,000 square feet.~~
- ~~2. There shall be no minimum lot area or width requirements for the conversion of existing buildings to mixed-use developments.~~

~~6.8.5~~ ~~6.8.4~~ Development Density

Pursuant to Section 6.5 of this Chapter, the maximum FAR and maximum unit density may be increased with the approval of a Conditional Use Permit, provided the mixed-use project specifically supports the purpose and intent of the Raincross District and is compatible with surrounding uses. The request for increased development intensity shall be reviewed as part of the Conditional Use Permit application for the mixed-use development project.

~~6.8.6~~ ~~6.8.5~~ Maximum Height

Pursuant to Section 6.5 of this Chapter, the maximum building height may be increased with the approval of a Conditional Use Permit, provided the mixed-use project specifically supports the purpose and intent of the Raincross District and is compatible with surrounding uses. The request for increased building height shall be reviewed as part of the Conditional Use Permit application for the mixed-use development project.

~~6.8.7~~ ~~6.8.6~~ Yard Setbacks

1. The setback requirements set forth in Section 6.5 of this Chapter shall apply to mixed-use development.

~~6.8.8~~ ~~6.8.7~~ Open Space

The following regulations shall determine the amount of required private and common open space:

1. New Projects:

- (a) Private Usable Open Space: At least 50% of the dwelling units in a project shall provide private usable open space, as defined in the Zoning Code, of a minimum of 50 square feet. All dwelling units in a project are encouraged to include private usable open space. A rectangle inscribed within each private usable open space shall have no dimension less than five feet. At least one exterior side shall be open above the level of railing or fencing. ~~Balcony/railing enclosures shall not be see-through.~~

- (b) Common Usable Open Space: At least 50 square feet of common usable open space, as defined in the Zoning Code, shall be provided per dwelling unit. Common usable open space may be divided into more than one area, however, each area shall be a minimum of 450 square feet and a rectangle inscribed within each shall have no dimension less than 20 feet. All required common open space shall be suitably improved for its intended purposes and all ~~lawn and~~ landscaped areas shall be provided with a permanent irrigation system to maintain such areas. The common open space may include courtyards, terraces and rooftops.

2. Conversions of existing buildings:

- (a) There shall be no minimum open space standards for conversions of existing buildings, however, every effort shall be made to achieve open space in all of the above categories to the extent feasible for the building being converted.

6.9 ADDITIONAL STANDARDS FOR MULTIFAMILY RESIDENTIAL DEVELOPMENT IN THE RAINCROSS DISTRICT

6.9.1 Applicability

The provisions of this section apply to multifamily residential development in the Raincross District.

6.9.2 Purpose

Historically, town centers included a mix of shops, offices, and restaurants at the street level and residences above. This pattern created a rich mix of uses and allowed urban activity to carry on well into the nighttime. In the 20th century, downtowns were increasingly zoned for one activity only, namely business, and were shutdown after six in the evening. In recent years, cities have once again begun to encourage residential uses within their downtowns and thus tap into the potential of increased economic activity by extending the usability of downtown resources into the nighttime. Residential uses are encouraged in the Raincross District both as a part of mixed use and multifamily residential developments.

Any multifamily structure shall conform to the standards and guidelines contained in section 6.5 and 6.6. Additional standards and guidelines are included in this section.



Examples of urban multi-family residential projects

6.9.3 Site Planning

Building Orientation

- (1) Buildings should have a strong street presence, with public entrances oriented towards the street.
- (2) Privacy between units should be maintained by locating balconies, porches, windows, etc., away from similar existing spaces in the adjacent units.

Setbacks

- (1) While no **front yard** setback is required in **all of the** Raincross District, except on parts of Mission Inn Avenue, the lowermost floor may be recessed to create space for landscape screening in multifamily residential developments.
- (2) On Mission Inn Avenue, the **front yard** setback area should be used for planting landscape screens to protect the privacy of the ground floor units.

Vehicular Access and Parking

- (1) Parking should be provided on site, located to the rear of the parcel wherever possible, in on-grade or underground structures or landscaped lots. Such parking areas should be fully screened from the street and security controls are permitted. Access should be limited to the minimum number of drives required to serve the required parking spaces. Except for town home projects with attached garages, all multi-family parking should be in efficient, multiple stall configurations.
- (2) If tuck-under parking is provided, the first floor of the residential units shall not occur more than four feet above the finished grade level. Parking may need to be depressed and occur behind the living spaces. Finished grades of the front entrances may be raised by up to four feet to accommodate this arrangement. These parking areas shall not be visible from the street.

Pedestrian Access

- (1) Pedestrian access to the first floor units should be via traditional residential front doors.

Open Space

The following is required for private and common open space for multifamily development:

- (1) **Private Usable Open Space:** Each dwelling unit shall have a minimum private usable open space as defined in the Zoning Code of 50 square feet. A rectangle inscribed within each private usable open space shall have no dimension less than five feet. At least one exterior side shall be open above the level of railing or fencing. Private open space features include: fenced yard/patio areas, balconies and roof gardens.
- (2) **Common Usable Open Space:** Inclusive of required setback yards, a minimum of 20 percent of the site area shall be designated and permanently reserved as usable common open space in multiple family developments with greater than 10 dwelling units. All required common open space shall be suitably improved for its intended purposes and all lawn and landscaped areas shall be provided with a permanent irrigation system to maintain such areas. Common open space areas include courtyards, terraces, rooftops, landscaping, picnic/barbecue areas, pools/spas, tennis/sport courts, clubhouse, totlots/playgrounds. Parking areas are not to be included in the calculation of open space.

Site Furniture

- (1) Outdoor pedestrian amenities should be provided in the 'Usable Open Space' areas required as noted in the section above.

Courtyards and Passages

- (1) 'Common Usable Open Space' areas should be provided as noted in the section on Open Space above.

6.9.4 Architecture

Style

- (1) Multiple family housing is unique in the sense that while an individual, private use, it is also a part of the urban fabric. Residents affect, and are affected by, street activity and provide "eyes on the street". The residential character of the individual units should be protected while conforming to the urban feel of the Raincross District.

Scale

- (1) Individual units should be articulated to diminish the massing of large structures and be compatible with the scale of surrounding development.
- (2) The mass and roof forms of buildings should be varied. In addition to porches, stoops and other entry elements such as bay windows, balconies and trellises are encouraged.
- (3) The street floor building level should be raised between two and four feet to protect the privacy of ground floor units.
- (4) Facades of multifamily buildings should be divided into shorter modules a maximum of 30 feet in width, to reflect the volumes of individual units within the building. This objective can be achieved with varied setbacks, vertical modulation, texture changes on the facade, porches and balconies.

6.9.5 Landscaping

Plant Types

- (1) Plantings of shrubs and flowering plants to add variety to the setback areas are encouraged.
- (2) Pathways and pergolas and trellises that are in character with the architectural style of development to add shade and interest are encouraged.

Relationship to Development

- (1) Common areas provided for the residents should be landscaped with planted areas, potted plants, seating, outdoor barbecue areas, pools and spas, etc.

6.9.6 Signage

Style

- (1) Traditional designs that reflect the building architecture are encouraged.
- (2) The signage for multifamily uses in this district should be discreet and subdued.

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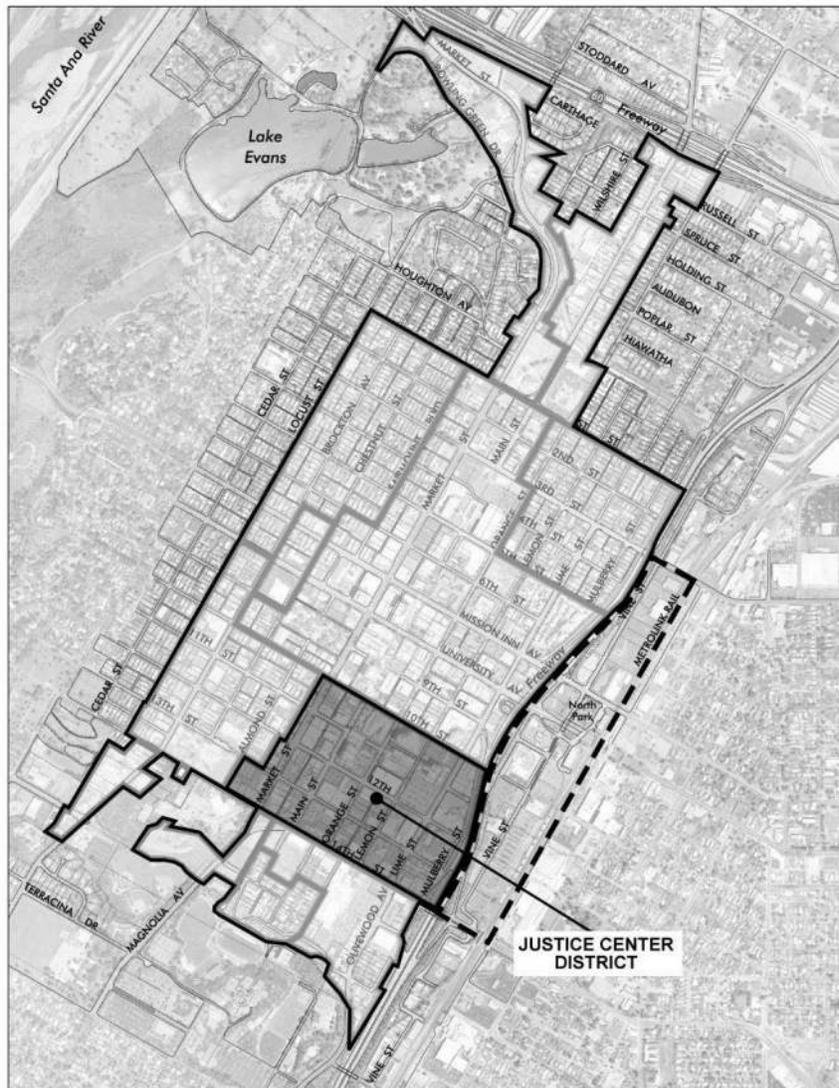
CHAPTER 7

JUSTICE CENTER DISTRICT

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This Chapter defines the land uses, development standards and design standards and guidelines for the Justice Center District. This Chapter is organized as follows:

- 7.1 Purpose
- 7.2 Permitted Uses
- 7.3 Conditionally Permitted Uses
- 7.4 Prohibited Uses
- 7.5 Development Standards for the Justice Center District
- 7.6 Design Standards and Guidelines for the Justice Center District



7.1 PURPOSE

The intent of the Justice Center District is to provide a center for the existing and future expansion of major judicial uses, including federal, appellate, state and county courts while simultaneously facilitating the expansion of the active, pedestrian-focused 24-hour mixed-use heart of Downtown Riverside. This District allows for a concentration of office, residential, mixed-use ~~uses~~ and retail uses to maintain Downtown as the employment center for the City of Riverside. Development intensity in the Justice Center District is the highest in the Downtown and is intended to encourage medium- to high-high density office, residential and mixed-use ~~uses~~ development to support and complement ~~the retail, cultural, entertainment and residential uses of~~ the nearby Raincross District. ~~The scale and grandeur of the Beaux-Arts style Riverside County Courthouse, located on Main Street, provides the opportunity to emphasize the judicial nature of this District by opening and reinforcing the view to this important architectural and historical structure from Market Street.~~

7.2 PERMITTED USES

The following uses are permitted in the Justice Center District:

- a) Banking establishments.
- b) Courts.
- c) Mixed-use development containing a vertical or horizontal mix of residential and non-residential uses.
- d) Offices, including but not limited to administrative, general, government and professional.
- e) Parks and open spaces.
- f) Personal service establishments, such as, but not limited to, barber or beauty shops, tailors, and shoe repair.
- g) Restaurants, including outdoor dining, ~~that are 1,500 square feet or less in size, or within lease space 1,500 square feet or less in size. Outdoor dining is subject to the standards established in the Zoning Code for outdoor dining.~~
- h) Retail sales.
- i) Temporary uses, pursuant to the standards established in the Zoning Code and consistent with the purposes of this District.
- j) Wireless communication facilities, pursuant to the Zoning Code.
- k) Accessory uses, incidental and subordinate to the principal permitted use.
- l) Other uses, not listed above, which are determined by the Planning Director to be similar to those listed above.

7.3 CONDITIONALLY PERMITTED USES

The following uses are permitted in the Justice Center District with a Minor Conditional Use Permit:

- a) Assemblies of People – Entertainment.
- b) Assemblies of People – Non-Entertainment.
- c) Brewpubs, Breweries, Microbreweries, Wineries (with off-sale retail and/or on-site tasting), Brew-on-premises (with tasting and/or retail sales of prepared beers) pursuant to the standards established in the Zoning Code.
- d) Farmer's market.

- e) Minor expansions to nonconforming uses with a previously approved Conditional Use Permit.
- f) On-sale of alcoholic beverages, pursuant to the standards established in the Zoning Code.
- g) Outdoor food preparation.

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~~h) Restaurants, including outdoor dining, that are more than 1,500 square feet in size, or within lease space greater than 1,500 square feet in size. Outdoor dining is subject to the standards established in the Zoning Code for outdoor dining.~~

~~i)h) Vapor recovery systems, pursuant to the Zoning Code.~~

~~j)j) Wireless communication facilities, pursuant to the Zoning Code.~~

The following uses are permitted in the Justice Center District with a Conditional Use Permit:

a) Catering businesses.

b) Child day care centers.

c) Educational facilities, including vocational schools.

d) Exercise facilities.

e) Expansion of nonconforming uses.

~~f) Hotels.~~

~~f)g) Parking structures and facilities with ground floor commercial uses.~~

~~g)h) Public uses not permitted as a matter of right.~~

~~h)i) Public utilities or installations.~~

~~i)j) Service stations (gas stations with minor repairs and/or food marts with or without the off-sale of beer and wine) on parcels adjacent to a freeway on or off ramp.~~

~~j)k) Wireless communication facilities, pursuant to the Zoning Code.~~

7.4 PROHIBITED USES

The following uses are prohibited in the Justice Center District:

a) Car wash.

b) Drive-thru establishments.

c) Motels.

~~d) Hotels.~~

~~e)d) Vehicle repair.~~

~~f)e) Vehicle sales and vehicle parts sales.~~

7.5 DEVELOPMENT STANDARDS FOR THE JUSTICE CENTER DISTRICT

All property in the Justice Center District shall be developed in accordance with the following standards. To ensure compatible development with the historic buildings in the Mission Inn Historic District, the maximum allowable height and maximum allowable density in this area is lower than for development in the remainder of the Justice Center.

7.5.1 Maximum Floor Area Ratio

A. Outside the Mission Inn Historic District:

The maximum floor area ratio (FAR) in the Justice Center District, outside of the Mission Inn Historic District shall be 5.0, except an increased FAR up to, but not exceeding, 6.0 may be permitted for the site amenity features listed in the following table. Amenities may be combined for increased FAR, except that in no case shall the floor area ratio exceed 6.0.

Amenity	Bonus
<p>Landscaped Courtyard or Atrium. A continuous area which is open and is accessible to the public, has a minimum area of 600 continuous square feet with a minimum dimension of 20 feet. Such courtyard or atrium shall be landscaped with greenery, statuary, water features, seating, or combination of the four.</p>	<p>Floor area increased four times the square footage contained within the courtyard.</p>
<p>Arcade or Pergola. A covered pedestrian passageway that connects two public streets; a public street and parking area or mall; or a public street and another arcade or pergola. The arcade or pergola shall be unobstructed, have a minimum width of 15 feet and a minimum vertical clearance of 12 feet and shall be situated at street grade. Arcades and Pergola located within the public right-of-way shall require an encroachment permit from the Public Works Department. Where an arcade or pergola is located within the public right-of-way, the minimum width may be reduced to conform with the width of the right-of- way.</p>	<p>Floor area increased three times the square footage contained within the arcade or pergola.</p>
<p>Other Amenities. Additional floor area ratio may be permitted in conjunction with modified amenities or amenities not listed above with the granting of a Conditional Use Permit, provided the proposed amenities specifically support the purpose and intent of the Justice Center District and are compatible with surrounding development and design. Specific floor area bonus shall be negotiated and the amenities must be acceptable to the City.</p>	<p>Floor area bonus negotiated in conjunction with the amenities provided and subject to the granting of a Conditional Use Permit.</p>

B. Within the Mission Inn Historic District:

The maximum floor area ratio for parcels within the Mission Inn Historic District shall be 3.0, except for the following:

- (1) Additional floor area ratio may be permitted for certain site amenity features set forth in the above table. Amenities may be combined for increased FAR, except that in no case shall the floor area ratio exceed 4.0.
- (2) In addition to the above amenity provision, floor area ratio may be increased up to 4.5 with the approval of a Conditional Use Permit, provided the proposed use specifically supports the purpose and intent of the Justice Center District and is compatible with surrounding development and design.

7.5.2 Minimum and Maximum Unit Density

A. The minimum dwelling unit density for mixed-use development containing a residential component shall be 60 dwelling units per acre.

B. The maximum dwelling unit density shall be 140 dwelling units per acre. The maximum unit density may be increased with the approval of a Conditional Use Permit.

~~7.5.2 Minimum Height~~

~~The minimum building height shall be 45 feet for at least 50% of the building footprint. Parcels in the Mission Inn Historic District are not subject to this requirement.~~

7.5.3 Maximum Height

A-C. Outside the Mission Inn Historic District:

The maximum building height in the Justice Center, outside the Mission Inn Historic District, shall be 140 feet, except for the following:

- (1) For parcels west of Market Street, the maximum building height shall be 60 feet.
- ~~(2) For parcels that have frontage on the east side of Market Street, the maximum building height shall be 60 feet to a depth of 50 feet from the front property line. The building height may step up to the maximum allowable building height at a point 50 feet from the front property line.~~
- ~~(3)~~(2) For parcels ~~outside the 50-foot setback~~ east of Market Street ~~defined above~~, building heights may exceed 140 feet with the granting of a Conditional Use Permit, provided that the maximum FAR is not exceeded.

B-D. Within the Mission Inn Historic District:

The maximum building height in the Justice Center, within the Mission Inn Historic District, shall be 60 feet, except for the following:

- (1) The maximum building height may be increased to a maximum height of 100 feet with

the approval of a Conditional Use Permit, provided the proposed use specifically supports the purpose and intent of the Justice Center District and is compatible with surrounding development and design.

7.5.4 Minimum Lot Size

The minimum lot size ~~for new development or~~ for the creation of new parcels shall be 20,000 square feet. |

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7.5.5 Front Yard Setback

The minimum ground-floor front yard setback shall be 10 feet; the maximum front yard setback shall be 30 feet. No parking is permitted in the front yard setback area. The front yard setback shall also apply to side and rear yards adjacent to a public street. ~~The following exception applies to front yard setbacks:~~

~~(1) For parcels that have frontage on Market Street, the minimum front yard setback shall be 15 feet; the maximum front yard setback shall be 30 feet.~~

7.5.6 Rear Yard Setback

No minimum rear yard setback is required, except for the following:

- ~~(1)~~ (1) Where the rear parcel line abuts the Almond Street District, the minimum rear yard setback shall be 15 feet if there is a public alley (distance from building to rear property line or alley easement), and 25 feet where there is no public alley.
- ~~(2)~~ When the project contains a residential component, the minimum rear yard setback shall be 5 feet if there is a public alley, and 15 feet where there is no public alley. This standard shall not apply to adaptive reuse, or conversion of existing buildings into a residential use.
- ~~(1)(3)~~ Where both of the above situations apply, the greater setback standard shall apply.

7.5.7 Interior Side Yard Setback

No minimum interior side yard setback is required, except where the side parcel line abuts the Almond Street District, the minimum interior side yard setback shall be 15 feet.

7.5.8 Parking

Refer to Chapter 16 for off-street parking requirements and standards.

7.6 DESIGN STANDARDS AND GUIDELINES FOR THE JUSTICE CENTER DISTRICT

7.6.1 District Character Defining Statement

The Justice Center is the legal and office center of Riverside and the surrounding region. Restaurant and retail uses are intended to be limited to those that are incidental to, and for the convenience of, the office and legal uses. The significant, signature buildings in this district include the Superior Court Building, Hall of Justice, Family Law Court, Bankruptcy Court, U.S. District Court, and the Catholic Church. This is primarily a district for contemporary buildings with historic references in detailing, massing, fenestration, and shapes. The district's historic buildings should be respected and complemented in any adjacent architecture. While taller buildings are anticipated in this district, respecting pedestrian scale is important. One example of how this can be accomplished is by the stepping of building heights from lower at the street to higher beyond. The above listed signature buildings should be used for inspiration regarding design, form, detailing, and site layout.



U.S. District Court of Appeals (12th and Lime Streets)



Bankruptcy Court (12th and Lime Streets)



Riverside County Courthouse (Main and 10th Streets)



Family Law Court (Main and 11th Streets)

7.6.2 Site Planning

Building Orientation

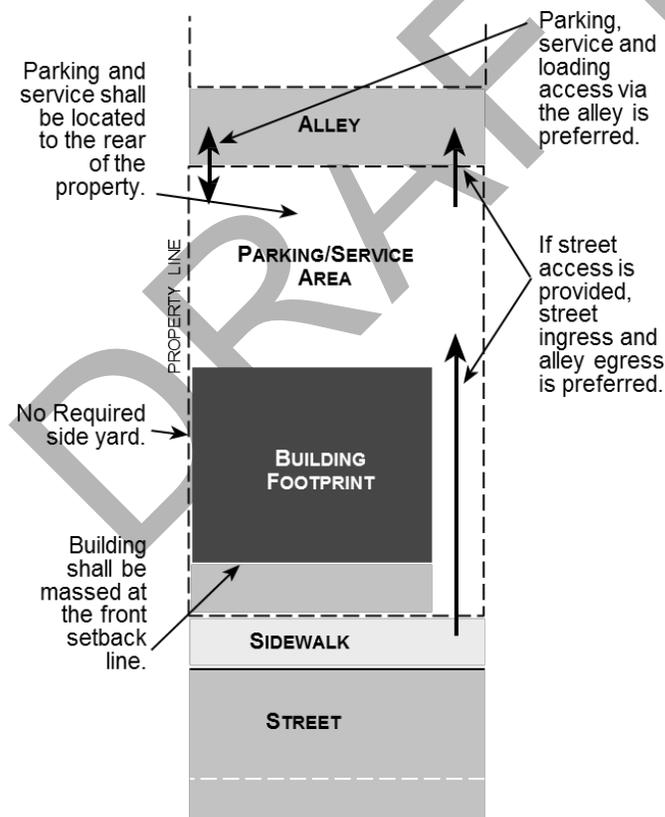
- (1) Building orientation should provide for at least one significant street entrance elevation. Other frontages should have a pedestrian scale and be softened by landscaping.

Setbacks

- (1) This district is characterized by varying depths of landscape setbacks around its signature buildings. Setbacks shall be relatively shallow, with breaks to provide public access.

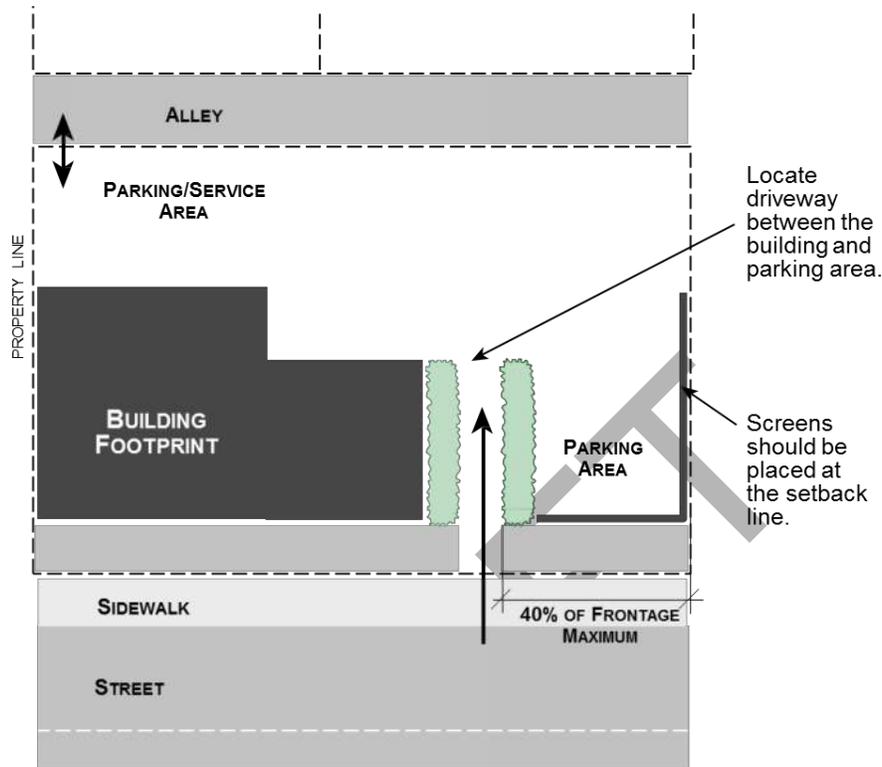
Vehicular Access and Parking

- (1) Parking shall typically be provided in parking garages with limited numbers of common access points.
- (2) ~~On~~ On-site parking (lots and structures) shall be located to the rear of the building ~~for~~ ~~parcel~~ widths less than 200 feet and accessed by alleyways wherever they exist.



Site Layout including driveway locations for parcels less than 200 feet in width.

- (3) For parcel widths in excess of 200 feet, parking lots and structures may occupy up to 40% of the parcel's street frontage.



Site Layout including driveway locations for parcels more than 200 feet in width.

Pedestrian Access

- (1) Pedestrian access may be directly from the street or via a courtyard having a significant frontage on a street.
- (2) Pedestrian access through or beneath buildings is appropriate in this district and is encouraged.

Site Furniture

- (1) Pedestrian amenities are encouraged, including benches, landscaped gathering areas, trash receptacles, etc. Design of improvements should be contemporary with traditional references.

Courtyards and Passages

- (1) Courtyards and pedestrian passages are encouraged to facilitate pedestrian circulation and to provide resting and gathering places. Design of improvements should be contemporary with traditional references.

7.6.3 Architecture

Style

- (1) Existing buildings should be restored/maintained in a style that reflects the original appearance of the building.
- (2) This district should exhibit a modern “formalness” befitting government and judicial uses. Government buildings should exhibit a formal character through contemporary architectural forms. The architectural style of new buildings should have a contemporary appearance but utilize elements that complement the historic character of Riverside. New buildings should be contemporary using the signature buildings as a source of design inspiration.

Scale

- (1) The portion of buildings and improvements with an orientation to pedestrian spaces should be at a pedestrian scale. Other parts of buildings, which do not include pedestrian spaces, may be high-rise.

Detailing

- (1) Detailing of existing buildings should be accurate to the original style of the building. New buildings should use detailing reflective of the signature buildings in the district.

Roof Design

- (1) Roof design should reflect/complement significant buildings in the area.

Colors and Materials

- (1) Muted earth tones and traditional materials should prevail, with brighter colors limited to trim areas. The Bankruptcy Court is a good example of this approach.

7.6.4 Landscaping

Plant Types

- (1) Plant types may be drawn from a broad plant palette, however, they should be types suitable for the climate and the exposure in which they are to be placed.

Scale

- (1) Pedestrian scale plantings should prevail, with larger plantings used as accents.

Relationship to Development

- (1) Plantings in courtyards should be at a pedestrian scale, accenting and complementing building architecture.
- (2) Plantings near taller building elements should be taller and/or larger in scale so as to relate to the view seen from a distance.

Hardscape

- (1) Paved areas may draw from a broad range of materials, designs, and finishes that are complementary to the building architecture.

7.6.5 Signs

Style

- (1) Contemporary designs that are low-key and complementary to the building's architecture are encouraged.

Scale

- (1) Signing should be an accent to the architecture. The overall effect should be low key and proportional to the building. Signs at the first floor should be the smallest and most low key. Signs at the top of the building should be large enough to be seen from a distance.

Type

- (1) Signs should be contemporary in nature. Acceptable types include channel letters, reverse channel letters, pegged-on metal letters, routed metal with push-through plastic letters. Plastic faced canister signs, wood signs, and painted signs are generally not appropriate.

7.6.6 Additional Standards for Mixed-Use Development

Refer to Chapter 6 (Raincross District), Section 6.9 for additional development standards applicable to mixed-use development in the Justice Center District.

7.6.6.7 Additional Design Standards and Guidelines

Refer to Chapter 15 for general design standards and guidelines that apply to the entire Downtown Specific Plan area.

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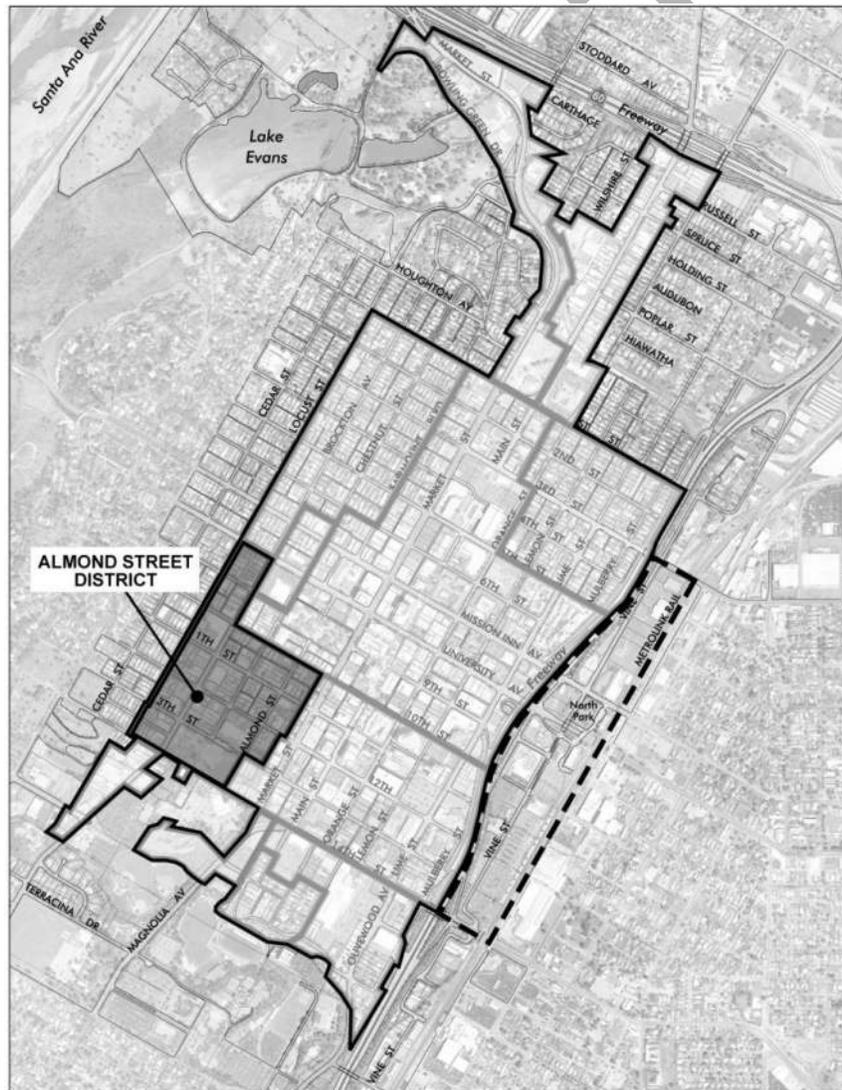
CHAPTER 8

ALMOND STREET DISTRICT

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This Chapter defines the land uses, development standards and design standards and guidelines for the Almond Street District. This Chapter is organized as follows:

- 8.1 Purpose
- 8.2 Permitted Uses
- 8.3 Conditionally Permitted Uses
- 8.4 Prohibited Uses
- 8.5 Development Standards for the Almond Street District
- 8.6 Design Standards and Guidelines for the Almond Street District
- 8.7 Additional Standards for Live/Work Units in the Almond Street District



8.1 PURPOSE

The intent of the Almond Street District is to provide for a variety of ~~low density residential uses, office uses, and the opportunity for live/work~~ and medical-related uses in an environment that is typically residential in character. This District serves as a transition area to the single-family residential neighborhood to the west, and therefore, is intended for ~~lower intensity transitional-scale development~~ to act as a buffer from higher-intensity development in the Justice Center District to the east. The preservation of significant residential structures in this District is encouraged, particularly along Locust Street, where the District borders an existing single-family residential zone. Single-family residential uses are allowed and encouraged to remain in this District, as well as adaptive reuse of these structures for ~~low density~~ office or live/work uses, which will maintain the historic, residential feel of the area. ~~Because of the single family residential character of this District, multiple lot development is discouraged~~ As Brockton Avenue south of University Avenue serves as the commercial spine of this part of Downtown, moderate-intensity mixed-use development is envisioned for properties fronting on Brockton Avenue. South of 14th Street, mixed-use development is envisioned to complement the Riverside Community Hospital campus.

As part of the Downtown Specific Plan/Mile Square West Survey Project, some of the structures in this area have been identified as having historic significance. ~~The introduction of new uses such as town homes would likely necessitate the removal of some existing structures. Any such transition of uses in this area is of particular concern with regard to impacts on historic resources.~~ In accordance with Title 20 of the Municipal Code, any development within this area must give careful attention to the preservation of these historic resources, either by retention on site or by relocation to an appropriate area.

8.2 PERMITTED USES

For parcels in the Almond Street District that have frontage on Locust Street, the following uses are permitted:

- a) Parks and open spaces.
- b) Single family dwelling units.
- c) Home occupations pursuant to the standards established in the Zoning Code.
- d) Wireless communication facilities, pursuant to the Zoning Code.

The following uses are permitted in the remainder of the Almond Street District:

- a) For sites that have frontage on Brockton Avenue, mixed-use development, including a vertical or horizontal mix of residential and nonresidential uses including but not limited to multi-family dwellings, retail, restaurant, and office uses, pursuant to the development standards set forth in Section 8.5 of this Chapter.
- a)b) Elsewhere in the Almond Street District, Existing, legally constructed multiple-family residential ~~structures that were originally constructed as, and for the purpose of multiple family dwelling units~~ uses. Such a permitted multiple-family residential structure may be rebuilt in similar size, at no greater density than the original use, if destroyed by fire or act of nature. Any renovation is subject to the design standards for multiple family dwellings set forth in Section 12.7 of Chapter 12.

- ~~b)c)~~ Home occupations pursuant to the standards established in the Zoning Code.
- ~~e)d)~~ Live/work units pursuant to the standards set forth in Section 8.7 of this Chapter.
- ~~d)e)~~ Medical and dental offices and ~~incidental~~ laboratories.
- ~~e)f)~~ Offices, general and professional.
- ~~g)~~ Parks and open spaces.
- ~~f)h)~~ Personal services.
- ~~g)i)~~ Pharmacies on parcels that have frontage on Brockton Avenue only.
- ~~j)~~ Single family dwellings units, including semi- and/or fully-attached units as defined in Chapter 18.
- ~~k)~~ Accessory Dwelling Units, pursuant to the standards established in the Zoning Code and applicable State law.
- ~~h)~~ Community & Economic Development Director, or his/her designee.

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- i) Temporary uses pursuant to the standards established in the Zoning Code and consistent with the purposes of this District.
- j) Wireless communication facilities, pursuant to the Zoning Code.
- k) Accessory uses, incidental and subordinate to the principal permitted use.
- l) Other uses, not listed above, which are determined by the Community & Economic Development Director, or his/her designee, Planning Director to be similar to those listed above.

8.3 CONDITIONALLY PERMITTED USES

The following uses are permitted in the Almond Street District with a Minor Conditional Use Permit:

- ~~a) Auxiliary dwelling units, pursuant to the standards established in the Zoning Code.~~
- b) Bed & breakfast inns.
- c) Minor expansions to nonconforming uses with a previously approved Conditional Use Permit.
- d) Vapor recovery systems, pursuant to the Zoning Code.
- e) Wireless communication facilities, pursuant to the Zoning Code.

The following uses are permitted in the Almond Street District north of 14th Street with a Conditional Use Permit:

- a) Assemblies of People – Non-Entertainment uses pursuant to the standards established in the Zoning Code.
- b) Catering businesses.
- c) Child day care centers.
- ~~e) Clubs and lodges.~~
- d) Educational facilities, including vocational schools.
- e) Expansion of nonconforming uses.
- ~~f) Places of worship.~~
- ~~g) Public uses not permitted as a matter of right.~~
- ~~h) Public utilities or installations.~~
- i) Wireless communication facilities, pursuant to the Zoning Code.

The following uses are permitted in the Almond Street District south of 14th Street with a Conditional Use Permit:

- a) Assemblies of People – Non-Entertainment uses pursuant to the standards established in the Zoning Code.
- b) Child day care centers.
- c) Expansion of nonconforming uses.
- d) Hospitals.
- e) Hotels.
- f) Long-term care facilities.
- g) Nonprofit family accommodations (e.g. Ronald McDonald House).
- h) Parking structures and facilities with ground-floor commercial and/or retail uses.
- i) Public uses not permitted as a matter of right.

- j) Public utilities or installations.
- k) Schools.
- a)l) Wireless communication facilities, pursuant to the Zoning Code.

8.4 PROHIBITED USES

The following uses are prohibited in the Almond Street District:

- a) Car wash.
- b) Drive-thru establishments.
- ~~e) Hotels.~~
- ~~d)c) Motels.~~
- ~~e)d) Service stations (gas station and minor repairs).~~
- ~~f)e) Vehicle repair shops and parts stores.~~
- ~~g)f) Vehicle sales and rentals.~~
- ~~h)g) Any use not specifically authorized.~~

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8.5 DEVELOPMENT STANDARDS FOR THE ALMOND STREET DISTRICT

All property in the Almond Street District shall be developed in accordance with the following standards:

8.5.1 Maximum Floor Area Ratio

A. For sites having frontage on Brockton Avenue, the maximum floor area ratio shall be 3.0.

A.B. ~~The maximum floor area ratio shall be 0.75~~For all other areas, the maximum floor area shall be 0.75.

8.5.2 Maximum Unit Density

A. For mixed-use development fronting on Brockton Avenue, the maximum dwelling unit density shall be 60 dwelling units per acre.

B. For residential development elsewhere in the District, the maximum dwelling unit density shall be controlled by the minimum lot size provisions of this Chapter.

~~8.5.28.5.3~~ Maximum Height

The maximum building height shall be ~~35-45~~ feet, ~~except within 165 feet of Locust Street, where the maximum building height shall be 35 feet.~~

~~8.5.38.5.4~~ Minimum Lot Size

The minimum lot size for new parcels shall be 5,000 square feet, except in the case of semi- and/or fully-attached single family developments where the minimum lot size shall be 1,800 square feet with a minimum lot width of 18 feet.

~~8.5.48.5.5~~ Front Yard Setback

There shall be no minimum front yard setback for sites having frontage on Brockton Avenue. Elsewhere in the Almond Street District, ~~T~~the minimum front yard setback shall be 20 feet. No parking is permitted in the front yard setback area, except that for single-family residential uses, parking is permitted in the front yard on an approved driveway. The setback area shall be landscaped or improved pursuant to the design standards set forth in Section 8.6 of this Chapter.

~~8.5.58.5.6~~ Rear Yard Setback

The minimum rear yard setback shall be 15 feet.

~~8.5.68.5.7~~ Side Yard Setback

The minimum interior side yard setback shall be 5 feet, except in the case of semi- and/or fully-attached single family developments where there is an interior side yard connection. In that case, no interior side setback is required. The minimum side yard setback adjacent to a street shall be 10 feet.

8.5.8 Additional Standards for Mixed-Use Development Fronting on Brockton Avenue

For all other development standards for mixed-use development fronting on Brockton Avenue, refer to Chapter 19.120 of the Zoning Code for the standards applicable to the Mixed Use Village Zone.

8.5.78.5.9 Parking

Refer to Chapter 16 for off-street parking requirements and standards.

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8.6 DESIGN STANDARDS AND GUIDELINES FOR ALMOND STREET DISTRICT

8.6.1 District Character Defining Statement

~~The character of the~~ The Almond Street District is defined by its existing historic single family residential buildings. ~~The overall look and feel of the area should remain that of a historic single family neighborhood. More contemporary construction that does not adhere to this vision should be considered nonconforming to the district's character. This is primarily a district for residential uses, including semi- and/or fully attached units, offices and live-work uses located in existing residential buildings.~~ The rhythm of small lot residential development is important, but and with the consolidation of individual lots to create larger projects ~~is not in character with this district.~~ Existing buildings should be retained, adapted and reused for residential or other permitted land uses where the house is structurally sound and not in a state of total disrepair. Where new construction is ~~unavoidable,~~ the district's historic residences should be respected and complemented in design, scale, and placement.



Existing residential use, Almond and 11th Streets



Adaptive reuse office, 3891 11th Street



Adaptive reuse office, 3891 10th Street



Adaptive reuse office, 4017 11th Street

8.6.2 Site Planning

Building Orientation

- (1) Buildings should have a traditional residential orientation to the street.
- (2) In new buildings, the first floor building levels should be set slightly above the sidewalk level to create a sense of transition from the public space to the private realm. Steps or ramps should be introduced either at the sidewalk or at the entry to the building.
- (3) Privacy between houses should be preserved by locating balconies, porches, windows, etc., away from similar spaces in the adjacent properties.

Setbacks

- (1) Front yard setbacks should be typical of the residential uses in the area, typically 20 feet.

Vehicular Access and Parking

- (1) Access should be via residential driveways with parking situated toward the rear and side of the lot. No parking is allowed in front of the building.
- (2) The width of driveways should not exceed 12 feet for residential uses and 20 feet for non-residential uses, while providing adequate room to maneuver vehicles.
- (3) Hollywood drives (middle planting strips) and open pavers are encouraged for residential uses.
- (4) The driveways should be at least 20 feet in depth from the garage face to the property line to allow on-site parking and reduce on-street parking.
- (5) Carports are allowed in homes built in an architectural style where carports are stylistically integral such as Craftsman style homes. Carports are discouraged in conjunction with non-residential uses.

Pedestrian Access

- (1) Pedestrian access should be via a traditional residential front door.
- (2) Front entries for residential uses should be clearly identified by using elements such as porches or stoops and non-residential uses should utilize similar residential-type entries where appropriate.
- (3) Entry walks from the sidewalk to the front door should reflect the residential character of the district. The width of the entry walks should not exceed five feet.

Site Furniture

- (1) As the primary character of the Almond Street District is residential, outdoor pedestrian amenities such as residential type seating should be provided in private open spaces such as front porches and rear yards.

Courtyards and Passages

- (1) Similar to site furniture above, gathering spaces in this district should generally be on front lawns and porches. Offices may provide private gathering spaces for their employees.

8.6.3 Architecture

Style

- (1) Existing buildings should be restored/maintained per the original appearance of the building.
- (2) New buildings should have a traditional residential style, typical of other residences in the area.
- (3) A consistent architectural style should be used for a building and the elements that relate to it, such as trellises, carports, roof forms, windows and detailing. While specific architectural styles are not dictated, several styles predominate in the Almond Street District (see signature buildings) and the other residential parts of Downtown Riverside and should provide inspiration to help maintain Riverside's unique character. Styles should not be replicated literally, but should be clearly reflected in a proposed project.
- (4) The rehabilitation of an older building should generally be done with an eye towards restoring the original architecture of the building, rather than covering it over with a new style.
- (5) An addition to an existing building should be designed to reflect and blend with the existing design of the structure.
- (6) The design of auxiliary structures (guesthouses, barns, sheds, detached garages, etc.) should be architecturally similar to the main structure.
- (7) Porches and roofs for a new building should be compatible with the existing patterns in the neighborhood.

Scale

- (1) Typical scale should be that of a modest one or two story single family residence.
- (2) New buildings should respect the overall massing scale of the neighborhood.
- (3) Long blank walls should be avoided.

Detailing

- (1) Detailing of existing buildings should be accurate to the original detailing of the building.
- (2) New buildings should use detailing reflective of the signature buildings in the district.

Roof Design

- (1) The typical roof in this area should be of a pitched design reflective of nearby residences.

Porches

Porches define a semipublic area that mediates between the public street and the private interior.

- (1) Existing porches should be preserved.
- (2) Enclosing porches or adding new porches to historic structures is strongly discouraged.
- (3) Elevated porches are strongly encouraged in new residential structures.
- (4) Special attention should be paid to its design as the porch should be an integral element of the building design and not appear added-on. Elements that need attention include roof slope, porch height and size, architectural details such as columns, balusters, balustrade and brackets.

Windows

- (1) All windows within a building should be related in operating type, proportion and trim. Unifying elements such as common headers and sills are encouraged.
- (2) Windows should either be inset at least three inches or framed to create shadow lines. Operable windows are encouraged to allow cross ventilation.
- (3) Place windows to promote privacy between properties.
- (4) Existing, historically correct windows should be preserved in place.

Garages

- (1) Single family residences, including semi- and/or fully-attached units, should have garages, preferably detached, located behind the residence. The garages may be front facing. If access via an alleyway is available, access from the alleys is encouraged.
- (2) Garage doors should be recessed into the garage door opening. Instead of a flat door, multi-panels should be used to break down the scale of the garage doors.
- (3) All garage doors should be designed to have an attractive appearance. Strong shadow lines should be created around the garage face by recessing the door six inches to a foot behind the adjacent building plane. Another option is to add a trellis that extends at least two feet over the garage face such that it adds strong shadows on the garage door face.
- (4) Carports may be used and require an architectural treatment that reflects the architectural character of the main structure.

Colors and Materials

- (1) Predominant materials should be wood siding, shingle roofing, and wood framed windows and doors.
- (2) Colors should be as appropriate to the style and period of the building. For example, Craftsman styles use muted earth tones, while Victorian styles use bright contrasting colors.

8.6.4 Landscaping

Plant Types

- (1) Plant types should be typical of residential plantings, with an emphasis on lawn in the front yard, foundation shrubbery, and limited use of small-scale trees as accents.

Scale

- (1) Pedestrian scale plantings should prevail, with larger plantings used as accents.

Relationship to Development

- (1) Plantings should be arranged to frame the architecture, provide a green carpet between the street and the building, and soften the view to the building foundation.
- (2) The public parkway along the street should be landscaped and maintained.
- (3) If a single-family residential structure is converted to an office use there should be increased landscaping and screening of structures and parking areas from adjacent dwellings.
- (4) If low garden structures (for example, a water fountain or sign base) are being installed, select and use materials that reflect the material palette of the house.

- (5) Pathways and pergolas and trellises that are in character with the architectural style of house to add shade and interest are encouraged.
- (6) Landscaped areas should be regularly maintained to prevent deterioration of the property.

Fencing/Walls

- (1) Any fencing along the sidewalk should be open, decorative fencing (picket, wrought iron) or low seat walls.
- (2) Side yard property lines may be delineated with plant materials, fences, or walls. The side yard fencing should not extend into the front setback.

Hardscape

- (1) Paving should be on a small scale and limited to walkways, driveways, and rear yard parking areas.
- (2) Varied paving textures and or elevation changes to define entrances, pedestrian areas, crosswalks, etc. should be used.

8.6.5 Signs

Style

- (1) Traditional designs that reflect the building architecture are encouraged.
- (2) Signage for office uses in this district should be discreet and subdued.

Scale

- (1) Scale should be proportional to a residential building and setting. The maximum size allowances of the Specific Plan should simply be a starting point, with stronger consideration given to the relationship between the scale of the sign and the building's architecture.

Type

- (1) Signs should be traditional in nature, typically constructed of wood. Plastic faced signs, metal signs, and painted signs are generally not appropriate.

8.6.6 Additional Design Standards and Guidelines

Refer to Chapter 13 for standards and guidelines for historic residential structures and to Chapter 15 for general design standards and guidelines that apply to the entire Downtown Specific Plan area.

8.7 ADDITIONAL STANDARDS FOR LIVE/WORK UNITS IN THE ALMOND STREET DISTRICT

8.7.1 Applicability

The provisions of this section apply to live/work units, as defined in Chapter 18: Definitions. These standards are in addition to the development standards set forth in Section 8.5 and the applicable design guidelines. Refer to Chapter 16 for parking standards and Chapter 17 for sign standards. The type of work permitted in a live/work unit shall be consistent with, or similar in nature to, the permitted uses in the Almond Street District, as defined in Section 8.2.

8.7.2 Purpose

The intent of this section is to provide for and make feasible the reuse of existing commercial or residential structures to accommodate live/work opportunities, as well as provide opportunities for the new development of buildings specifically designed and constructed to provide live/work units.

8.7.3 Floor area requirements

The minimum floor area of a live/work unit shall be 900 square feet.

8.7.4 Internal layout

All living space within the live/work unit shall be contiguous with, and an integral part of the working space, with direct internal access between the two areas.

8.7.5 Occupancy and employees

At least one of the full-time workers of the live/work unit shall reside in the unit. The residential area shall not be rented separately from the working space. The business activity occupying the live/work unit may utilize employees in addition to residents as necessary.

8.7.6 Business License Tax Certificate

A business license-tax certificate shall be obtained in compliance with the Municipal Code for business activities conducted within the live/work unit.

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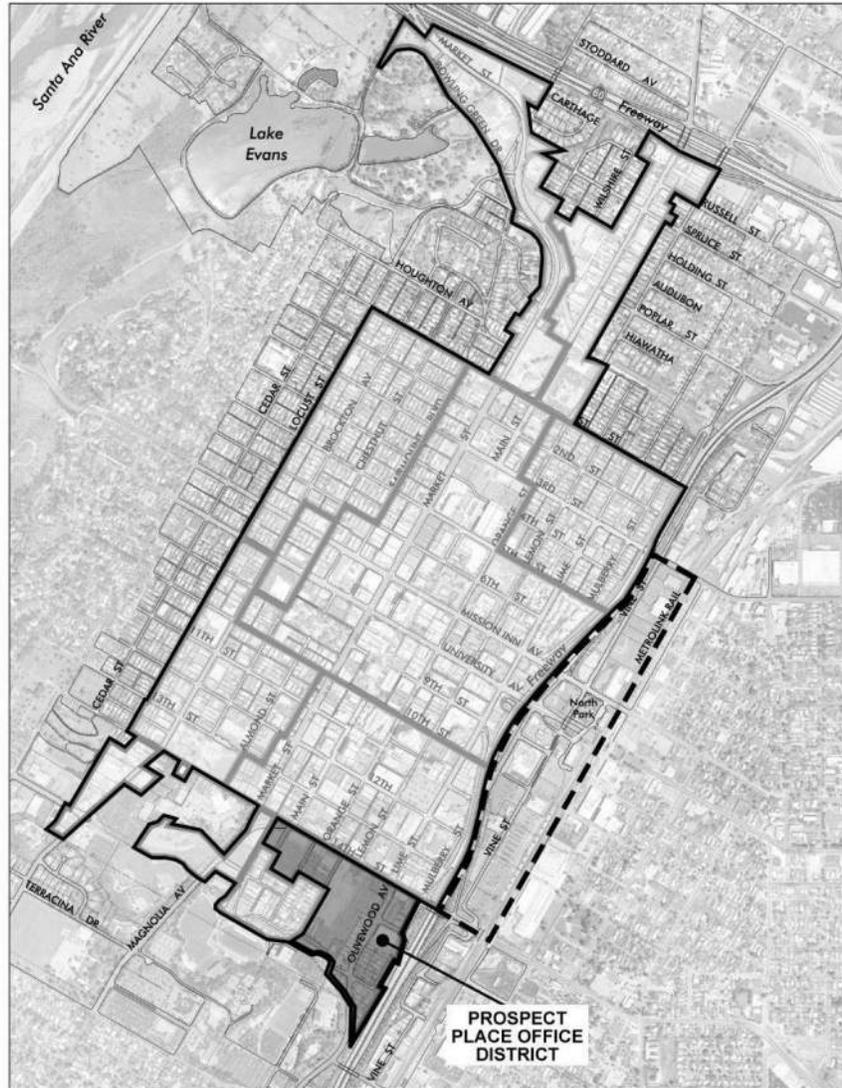
CHAPTER 9

PROSPECT PLACE OFFICE DISTRICT

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This Chapter defines the land uses, development standards and design standards and guidelines for the Prospect Place Office District. This Chapter is organized as follows:

- 9.1 Purpose
- 9.2 Permitted Uses
- 9.3 Conditionally Permitted Uses
- 9.4 Prohibited Uses
- 9.5 Development Standards for the Prospect Place Office District
- 9.6 Design Standards and Guidelines for the Prospect Place Office District
- 9.7 Additional Standards for Live/Work Units in the Prospect Place Office District



9.1 PURPOSE

The intent of the Prospect Place Office District is to provide for a variety of high quality office uses, as well as the expansion of the Press-Enterprise facility. Special attention will be given to development along 14th Street and Olivewood Avenue, which serve as entry points into the Downtown. The preservation of significant historic structures in this District is an important priority and is encouraged through continued residential use or adaptive reuse of the structures for ~~low density~~ office uses. Relocation of the smaller historic residential structures in this District is encouraged when preservation is not feasible for upgraded development or adaptive reuse. ~~New development should be compatible with the historic character of this District.~~

9.2 PERMITTED USES

The following uses are permitted in the Prospect Place Office District:

- a) Banking establishments.
- ~~b) Business supply retail uses such as, but not limited to, office supplies, equipment rental and repair, photocopy shops, blueprinting, etc. for parcels that have frontage on 14th Street only.~~
- ~~c) Existing multiple family residential structures that were originally constructed as, and for the purpose of multiple family dwelling units. Such a permitted multiple family residential structure may be rebuilt in similar size, at no greater density than the original use, if destroyed by fire or act of nature. Any renovation is subject to the design standards for multiple family dwellings set forth in Section 8.8 of Chapter 8.~~
- ~~d)~~ Existing single-family residences.
- ~~e)~~ Existing, legally established, free-standing car washes. Such a permitted car wash may be rebuilt in similar size and configuration as the original use if destroyed by fire or act of nature.
- ~~f)~~ Home occupations pursuant to the standards established in the Zoning Code.
- ~~g)~~ Live/work units pursuant to the standards set forth in Section 9.7 of this Chapter.
- f) Multi-family dwellings, including as part of a vertical or horizontal mixed-use development containing both residential and non-residential components.
- ~~h)~~ Offices, general and professional.
- ~~i)~~ Parks and open spaces.
- ~~j)~~ Publishing, printing and other uses associated with commercial printing.
- ~~k)~~ Radio, television, Internet, communications, and other information uses.
- ~~l)~~ Restaurants, including outdoor dining, that are 1,500 square feet or less in size, or within lease space 1,500 square feet or less in size. Outdoor dining is subject to the standards established in the Zoning Code for outdoor dining.
- l) Retail sales.
- m) Temporary uses pursuant to the standards established in the Zoning Code and consistent with the purposes of this District.
- n) Wireless communication facilities, pursuant to the Zoning Code.
- o) Accessory uses, incidental and subordinate to the principal permitted use.
- p) Other uses, not listed above, which are determined by the Planning Director, Community & Economic Development Department Director, or his/her designee, to be similar to those listed above.

9.3 CONDITIONALLY PERMITTED USES

The following uses are permitted in the Prospect Place Office District with a Minor Conditional Use Permit:

- a) Minor expansions to nonconforming uses with a previously approved Conditional Use Permit.
- b) On-sale alcoholic beverages, pursuant to the standards established in the Zoning Code.
- c) Outdoor food preparation.
- d) Vapor recovery systems, pursuant to the Zoning Code.
- e) Wireless communication facilities, pursuant to the Zoning Code.

The following uses are permitted in the Prospect Place Office District with a Conditional Use Permit:

- a) Catering businesses.
- b) Child day care centers.
- c) Educational and vocational schools.
- d) Exercise facilities.
- e) Expansion of nonconforming uses.
- f) Hotels.
- g) Public uses not permitted as a matter of right.
- h) Public utilities or installations.
- i) Service stations (gas station and minor repairs), with or without an existing car wash, on parcels adjacent to a freeway on or off ramp.
- j) Wireless communication facilities, pursuant to the Zoning Code.

9.4 PROHIBITED USES

The following uses are prohibited in the Prospect Place Office District:

- a) Car wash.
- b) Drive-thru establishments.
- ~~e)~~ ~~Hotels.~~
- ~~d)c)~~ Motels.
- ~~e)d)~~ Vehicle repair shops and parts stores.
- ~~f)e)~~ Vehicle sales and rentals.
- ~~g)f)~~ Any use not specifically authorized.

9.5 DEVELOPMENT STANDARDS FOR THE PROSPECT PLACE OFFICE DISTRICT

All property in the Prospect Place Office District shall be developed in accordance with the following standards:

9.5.1 Maximum Floor Area Ratio

The maximum floor area ratio (FAR) shall be 1.5, except for the following:

- (1) For ~~parcels/sites~~ that have frontage on 14th Street and Olivewood Avenue, the maximum floor area ratio shall be 4.0.

9.5.2 Maximum Unit Density

The maximum dwelling unit density shall be 60 dwelling units per acre. The maximum dwelling unit density may be increased with the approval of a Conditional Use Permit, provided the maximum FAR is not exceeded.

9.5.29.5.3 Maximum Height

The maximum building height shall be 35 feet, except for the following:

- (1) For parcels that have frontage on 14th Street and Olivewood Avenue, the maximum building height shall be 80 feet to a depth of 300 feet from the front property line, ~~except that development within the entire block bounded by 14th Street, State Highway 91, Prospect Avenue and Orange Grove Avenue may have a maximum building height of 80 feet, subject to approval of a site plan to ensure compatibility with surrounding historic residential structures.~~ The maximum building height in these areas may exceed 80 feet with the granting of a Conditional Use Permit, provided the maximum FAR is not exceeded.
- (2) ~~For parcels w~~Within the defined areas above, building heights may exceed 80 feet with the granting of a Conditional Use Permit, provided that the maximum FAR is not exceeded100 feet of the Residential District, the maximum building height shall be 45 feet.

9.5.39.5.4 Minimum Lot Size

The minimum lot size for new parcels shall be 20,000 square feet.

9.5.49.5.5 Front Yard Setback

The minimum front yard setback shall be 15 feet, except for the following:

- (1) The minimum front yard setback along 14th Street shall be 10 feet.

No parking is permitted in the front yard setback area, except that for single-family residential uses, parking in the front yard on an approved driveway is permitted. The setback area shall be landscaped

or improved pursuant to the design standards set forth in Chapter 15 of this Plan. The front yard setback shall also apply to side and rear yards adjacent to a street.

9.5.59.5.6 Rear Yard Setback

No minimum rear yard setback is required, except for the following:

- (1) Where the rear parcel line abuts the ~~Prospect Place Historic District~~Residential District, the minimum rear yardsetback shall be 15 feet if there is a public alley (distance from building to rear property line oralley easement), and 25 feet where there is no public alley.

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9.5.69.5.7 Interior Side Yard Setback

No minimum interior side yard setback is required, except where the side parcel line abuts the ~~Prospect Place Historic~~Residential District, the minimum interior side yard setback shall be 15 feet.

9.5.8 Additional Standards for Multi-Family and Mixed-Use Development

For additional development standards for multi-family and mixed-use development in the Prospect Place Historic District not otherwise specified in this Chapter, refer to Sections 6.8 and 6.9 of Chapter 6 (Raincross District).

9.5.79.5.9 Parking

Refer to Chapter 16 for off-street parking requirements and standards.

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9.6 DESIGN STANDARDS AND GUIDELINES FOR THE PROSPECT PLACE OFFICE DISTRICT

9.6.1 District Character Defining Statement

The Prospect Place Office District is an area with a split personality. In the area of 14th Street and throughout the Press-Enterprise property (bounded by Fourteenth, Prospect, Olivewood, and Orange Grove), the area's character is defined by large-scale office buildings and the intense publishing activities of the Press-Enterprise. The key signature buildings in this area include the School District building and the Bank of America building. These buildings help define this as an area of contemporary office buildings with a very 'corporate' look. The Press-Enterprise plans an expansion within its larger property that is anticipated to fill-out and further define this corporate presence. The existing church building on the Press Enterprise property, while not in character with the balance of the area, is an important historic reference that should remain as a part of the heritage of the area.



School District (14th Street & Olivewood Avenue)



Bank of America (Main and 14th Streets)



Twogood Residence, 3410 Prospect Avenue



McIntyre Residence, 4586 Olivewood Avenue

East of Olivewood and south of Prospect Place, the area's character is defined by greenery and low intensity residential uses. The signature buildings for this area are the Mission Revival Twogood residence and the Victorian style McIntyre and Sweatt residences, all clustered at the corner of Prospect Place and Olivewood Avenue. These historic residences should be preserved and any future development elsewhere in this area should use these signature buildings as a guide for scale, proportions, detailing, and general style. Preservation of the other residences in this area is also encouraged, whether for adaptive reuse as offices or maintenance as residences. The general character of this Olivewood corridor should be that of a green transitional area between the parklike Riverside Community College campus environment and the more urban 'Downtown' experience in the vicinity of 14th Street. For standards and guidelines applicable to these historic residences, please refer to Chapter 13: Residential Districts.

9.6.2 Site Planning

Building Orientation

- (1) In the 14th Street/Press-Enterprise area, buildings should orient to the street with no intervening parking.
- (2) In the Olivewood corridor, buildings should have a traditional residential orientation to the street.

Setbacks

- (1) In the 14th Street/Press-Enterprise area, buildings should have a small setback to provide a green 'apron' to the street.
- (2) In the Olivewood corridor, setbacks should be typical of the residential uses in the area, typically 20 feet.

Vehicular Access and Parking

- (1) In the 14th Street/Press-Enterprise area, access should be via shared driveways, with parking concentrated toward the interior of the site.
- (2) In the Olivewood corridor, access should be via residential driveways with parking situated toward the rear of the lot.

Pedestrian Access

- (1) In the 14th Street/Press-Enterprise area, pedestrian access should be from the parking areas to the rear.
- (2) In the Olivewood corridor pedestrian access should be via a traditional residential front door.

Site Furniture

- (1) In the 14th Street/Press-Enterprise area, pedestrian amenities should be concentrated in courtyards designed to serve the employees of the building.
- (2) In the Olivewood corridor, pedestrian amenities should typically be limited to residential type seating on front porches and in rear yards.

Courtyards and Passages

- (1) In the 14th Street/Press-Enterprise area, courtyards and passages are encouraged.

- (2) In the Olivewood corridor, where courtyards and pedestrian passages are not typical, gathering spaces should generally be on front lawns and porches.

9.6.3 Architecture

Style

- (1) In the 14th Street/Press-Enterprise area, architecture should be contemporary in nature with distinct references to unique Riverside symbols and characteristics, such as the Raincross symbol and the predominance of various forms of Spanish revival architecture.
- (2) In the Olivewood corridor, existing buildings should be restored/maintained per the original appearance of the building. New buildings should have a traditional residential style, typical of other residences in the area.

Scale

- (1) In the 14th Street/Press-Enterprise area the scale of development will be similar to the Justice Center to correspond with the massing in the Justice Center across the street.
- (2) In the Olivewood corridor, the scale should be that of a modest one or two story single family residence to blend with the residential uses in the Twogood Neighborhood Conservation Area and the Prospect Place Residential Neighborhood.

Detailing

- (1) In the 14th Street/Press-Enterprise area, detailing should be interpretive of the signature buildings in the Raincross District.
- (2) In the Olivewood corridor, detailing of existing buildings should be accurate to the original detailing of the building.
- (3) New buildings in this area should use detailing reflective of the signature buildings in the district.

Roof Design

- (1) In the 14th Street/Press-Enterprise area, roof styles may vary, depending upon the nature of the architecture.
- (2) In the Olivewood corridor, the typical roof style should be a pitched design reflective of nearby residences.

Colors and Materials

- (1) In the 14th Street/Press-Enterprise area, colors should be typical of formal corporate architecture, including understated colors such as gray, white, black, and rich earth tones. Materials should reflect the durable image of the buildings, including glass curtainwall, brick, marble, cut stone, and smooth cement or plaster.
- (2) In the Olivewood corridor, predominant materials should be wood siding, shingle roofing, and wood framed windows and doors. Colors in this area should be appropriate to the style and period of the building. For example, Craftsman styles should use muted earth tones, while Victorian styles should use bright contrasting colors.

9.6.4 Landscaping

Plant Types

- (1) In the 14th Street/Press-Enterprise area, plant types may be drawn from a broad palette, however, they should be suitable for the climate and the exposure in which they are to be placed.
- (2) In the Olivewood corridor, plant types should be typical of residential plantings, with an emphasis on lawn in the front yard, foundation shrubbery, and limited use of small-scale trees as accents.

Scale

- (1) In the 14th Street/Press-Enterprise area, pedestrian scale plantings should prevail, with larger plantings used as accents.
- (2) In the Olivewood corridor, plant types should be typical of residential plantings, with an emphasis on lawn in the front yard, foundation shrubbery, and limited use of small-scale trees as accents.

Relationship to Development

- (1) In the 14th Street/Press-Enterprise area, plantings in courtyards should be at a pedestrian scale, accenting and complementing building architecture. Plantings near taller building elements should be taller and/or larger in scale so as to relate to the view seen from a distance.
- (2) In the Olivewood corridor, plantings should be arranged to frame the architecture, provide a green carpet between the street and the building, and soften the view to the building foundation.

Hardscape

- (1) In the 14th Street/Press-Enterprise area, paved areas should draw from a broad range of materials, designs, and finishes that are complementary to the building architecture.
- (2) In the Olivewood corridor, paving, typically on a small scale, should be limited to walkways, driveways, and rear yard parking area.

9.6.5 Signs

Style

- (1) In the 14th Street/Press-Enterprise area, contemporary designs that are low-key and complementary to the building's architecture are encouraged.
- (2) In the Olivewood corridor, traditional designs that reflect residential building architecture are encouraged.

Scale

- (1) In the 14th Street/Press-Enterprise area, signing should be an accent to the architecture. The overall effect should be low key and proportional to the building. Signs at the first floor should be the smallest and most low key. Signs at the top of the building need to be large enough to be seen from a distance.

- (2) In the Olivewood corridor, sign scale should be proportional to a residential building and setting. The maximum size allowances of the Specific Plan should simply be a starting point, with stronger consideration given to the relationship between the scale of the sign and the building's architecture.

Type

- (1) In the 14th Street/Press-Enterprise area, signs should be contemporary in nature. Acceptable types include channel letters, reverse channel letters, pegged-on metal letters, routed metal with push-through plastic letters. Plastic faced canister signs, wood signs, and painted signs are generally not appropriate.
- (2) In the Olivewood corridor, signs should be traditional in nature, typically constructed of wood. Plastic faced signs, metal signs, and painted signs are generally not appropriate.

9.6.6 Additional Design Standards and Guidelines

Refer to Chapter 15 for general design standards and guidelines that apply to the entire Downtown Specific Plan area.

9.7 ADDITIONAL STANDARDS FOR LIVE/WORK UNITS IN THE PROSPECT PLACE OFFICE DISTRICT

9.7.1 Applicability

The provisions of this section apply to live/work units, as defined in Chapter 18: Definitions. These standards are in addition to the development standards set forth in Section 9.5 and the applicable design guidelines. Refer to Chapter 16 for parking standards and Chapter 17 for sign standards. The type of work permitted in a live/work unit shall be consistent with, or similar in nature to, the permitted uses in the Prospect Place Office District, as defined in Section 9.2.

9.7.2 Purpose

The intent of this section is to provide for and make feasible the reuse of existing commercial or residential structures to accommodate live/work opportunities, as well as provide opportunities for the new development of buildings specifically designed and constructed to provide live/work units.

9.7.3 Floor area requirements

The minimum floor area of a live/work unit shall be 900 square feet.

9.7.4 Access to units

When more than one live/work unit is proposed within a single building, each live/work unit shall be separated from other live/work units and other uses in the building. Access to individual units shall be from common access areas, corridors, or hallways.

9.7.5 Internal layout

All living space within the live/work unit shall be contiguous with, and an integral part of the working space, with direct internal access between the two areas.

9.7.6 Occupancy and employees

At least one of the full-time workers of the live/work unit shall reside in the unit. The residential area shall not be rented separately from the working space. The business activity occupying the live/work unit may utilize employees in addition to residents as necessary.

9.7.7 Business License

A business license shall be obtained in compliance with the Municipal Code for business activities conducted within the live/work unit.

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CHAPTER 16

PARKING STANDARDS

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This Chapter presents recommendations for Downtown parking to meet future demand and also establishes the vehicle parking standards and guidelines for bicycle parking for the Downtown Specific Plan area. The Chapter is organized as follows:

- 16.1 Context
- 16.2 Parking Needs and Requirements
- 16.3 Standards for Off-Street Parking and Loading
- 16.4 Parking Strategies
- 16.5 Bicycle Parking Guidelines

16.1 CONTEXT

Just prior to commencing the Downtown Specific Plan effort, Wilbur Smith Associates was hired by the City of Riverside to conduct a separate downtown parking study focusing on existing parking conditions, future parking demand and parking operations and management. The Comprehensive Downtown Parking (CDP) Study by Wilbur Smith Associates includes the area bounded by Fourteenth Street, Market Street, Third Street and Lime Street. Although the CDP study area does not correspond to either the Downtown Specific Plan area as a whole or any of its Districts, there is enough similarity to draw basic conclusions. However, it should be noted that since the CDP Study has a different focus from the specific plan, it needs to be considered as a complementary study and used in conjunction with the specific plan.

The CDP Study has identified a total of 9,658 parking spaces, of which 1,625 are on-street and 8,033 spaces are off-street. The same study identified the current peak parking demand in the study area at 6,715 spaces, indicating an overall existing surplus of parking. The off-street spaces are almost equally divided among City, County and private spaces, and 48% are available for use by the general public, while the remaining 52% are for private use or are reserved spaces. Public facilities are generally dispersed throughout the downtown area, with short walking distances between parking locations and ultimate destinations (typically only one or two blocks).

Currently, the key issues regarding parking in the downtown are as follows:

- Off-street parking demand in the downtown area is generally below capacity and there is potential to shift on-street long term parking to nearby lots and structures.
- There is a general shortage of on-street spaces for visitors and shoppers along Mission Inn Avenue and University Avenue. However, the nearby lots and structures are underutilized.
- One reason that parking lots and structures are underutilized is that they are not clearly identified and visible to the infrequent downtown visitor, including businesspersons.
- Priority tends to be given to permit/employee parking, rather than visitor parking. Permit parking is often located in the most convenient ground/lower floor locations of parking structures forcing visitors to circulate to the upper levels.
- Some parking lots are confusing in regard to whether the lot is available for public parking and whether there is a cost associated with parking.
- The older garages are intimidating and outdated with poor lighting and narrow access aisles.

- The current parking code requires parking supply for buildings to be located on-site. This is an impediment to new land uses in downtown.

The CDP Study provides an assessment of current and future conditions and the reader is, therefore, referred to the CDP Study for a detailed assessment of downtown parking.

An earlier study, Magnolia/Market Corridor Study by Moule & Polyzoides with The Mobility Group, also addressed parking in the downtown area. This study discussed and recommended a “Park Once” policy, described as follows:

“In order to enhance the effectiveness of transit and reduce the general reliance on automobiles, land uses and buildings need to be focused and concentrated into villages or neighborhood nodes. Multiple use buildings, higher population densities, and traditional village centers create walkable districts where people can meet most of their daily needs without using their cars. The principle transportation within each village or neighborhood node should be walking. By placing buildings close to the center of the district, and close to the street, people can walk between different buildings, to or from transit stops, and around the village without having to move their cars. This concept is called ‘Park Once’.”

“By establishing a Park Once program and through shared parking opportunities, the City and property owners can create fewer parking spaces yet still maintain a high number of users. If one parking spot can serve three different businesses, then the overall amount of required parking could be reduced relative to standard dispersed suburban development. On-street parking is promoted because it is often the most conveniently located, and additional parking can be accommodated in shared parking lots behind buildings in a village center. Traffic will often be reduced as well, since a high proportion of traffic in suburban style commercial areas is traffic circulating between parking spaces for visits to different buildings within the same area.”

The Magnolia/Market Corridor Study also made the following recommendations regarding parking in the “Downtown District”:

“Market Street is dominated by parking lots and parking structures and has many substandard retail and commercial activities. The City’s policy of concentrating parking on the edges of downtown needs to be re-evaluated and better dispersed. While parking uses should not face onto the major retail streets of the Raincross District, neither should parking be concentrated along streets on the edge of downtown. A more dispersed and less concentrated alternative would improve the district and better promote a Park Once program throughout the City.”

“An important aspect of a park once program is to evenly disperse parking throughout a neighborhood or district. Instead of consolidating parking structures along Market Street, they should be dispersed throughout downtown. This would mean that building code requirements should be changed to allow parking to be located off site within a certain distance from the specific use. Better-dispersed parking allows for a well-integrated Park Once approach for the City thereby promoting greater pedestrian activity and use. Retail uses should also be located at first floors of all parking structures to maintain pedestrian continuity and use.”

16.2 PARKING NEEDS AND REQUIREMENTS

The future parking supply needs for downtown will depend on the amount of new development, the demand for parking, and the requirements of the City's Parking Code.

16.2.1 Future Supply Needs

The CDP Study provides an estimate of the parking needs for the development opportunities identified in Chapter 21: Implementation. This analysis indicates that the parking requirement for these projects would be in the range of 3,360 to 4,460 spaces, depending on their ultimate configuration. These estimates include adjustments for estimated shared use potential and replacement of parking spaces that would be displaced by the development projects.

The analysis also indicates that the peak demand for parking in the study area will increase from its current level of 6,715 spaces to somewhere in the range of 10,070 to 11,175 spaces. Some of this need could be accommodated by the existing surplus of supply while some will need to be accommodated by the provision of additional spaces. The CDP Study provides a detailed discussion of these needs.

16.2.2 Future Supply Locations

With respect to additional parking for the development opportunity sites, parking spaces for the hotel/convention center expansion will need to be constructed in close proximity to the Convention Center. Other spaces are less location specific. A number of potential sites for new parking structures (of varying sizes) have been identified as follows and are shown in Figure 16A:

- The half-block bounded by 5th and 6th Streets, east of Market Street (development opportunity Site #3)
- The half-block west of Lemon Street, between University Avenue and 9th Street (Development opportunity Site #126). Retail uses should be emphasized along University Avenue.
- The northwest corner of Orange Street and 9th Street.
- The southwest corner of Orange Street and 9th Street
- Between Lime Street and SR-91 between 10th and 11th Streets
- Between Lime Street and SR-91 between 11th and 12th Streets

These sites should be able to provide the additional non-hotel related spaces that will be needed for the identified development opportunities. Depending on the sizes of the structures these sites could also potentially provide additional spaces.



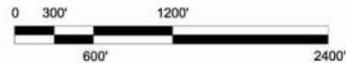
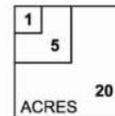
Figure 16A
Potential Parking Structure Locations
 Downtown Specific Plan

- LEGEND**
- Parcel Boundary
 - Public Right-of-Way Boundary

- Potential Parking Structure Locations
- Downtown Specific Plan Boundary
- Riverside Marketplace



in association with
 CBA, Inc.
 EPT Landscape Architecture
 Isenberg & Associates, Inc.
 Keyser Marston Associates, Inc.
 The Mobility Group
 Myra L. Frank & Associates, Inc.



16.2.3 Basis for Modifications to the Parking Code

Most City Parking Codes, including Riverside's current code, set out parking ratio requirements for individual stand-alone land uses. While this is appropriate for most areas of the City, it is not appropriate for downtown areas for the following reasons:

- There is much more interaction between land uses in downtown areas, as people walk from one building to another.
- There is usually more on-street parking in downtown areas. (For example, approximately 17% of the parking in downtown Riverside is on-street)
- More people ride transit to downtown because transit service (both routes and service frequency) tends to be focused on downtown
- Parking costs are usually higher in downtown, so more people rideshare.
- The peak parking demand for different uses tends to occur at different times of the day, so some parking supply can be shared by multiple uses.
- Greater concentration of ride share.

Although there are a variety of land uses requiring parking in the downtown, the opportunities for shared parking between many of those uses (particularly in the Raincross District) means that fewer spaces need to be provided in total. This is supported by factual evidence from numerous cities across the country. For example, studies in downtown communities as diverse as Manhattan Beach, California; Mountain View, California; Rockville, Maryland; and Boston's Wharf, Baltimore have shown that the actual peak demand for parking ranges from 47% to 62% of the Parking Code requirements, and averages 54% of the Code requirements. The recent CDP Study of Downtown Riverside indicated that current parking demand is about 56% of the overall code requirement for the downtown area. This evidence indicates that typical Parking Code requirements do not apply in downtown areas and that if applied they will lead to a significant oversupply of parking.

Exploring this further, downtown uses can be divided into two categories. The first type - base uses - are uses that bring people downtown for long periods of the day and are the primary drivers of parking demand. Such uses include office uses that bring workers into the downtown for many hours of the day. Hotels are also base uses because guests generate a demand for parking (particularly overnight) and meeting rooms are a principal destination requiring parking. Some visitors to downtown also visit specific uses such as restaurants and retail uses, for which adequate parking needs to be provided.

The second type - other uses - are those that many people visit because they are already in downtown for another reason and they walk to these uses. These include some visitors to retail and restaurant uses. While some people will come to downtown just to visit these uses, and thus require parking, others will already be in downtown, at work or staying at a hotel, and will walk to a store or a restaurant (or from a nearby residential area) and thus will not need a separate place to park. Other uses such as cinemas and theaters, tend to create peak parking demands in the evenings and weekends and can therefore share parking when demand for office parking is negligible.

16.2.4 Parking Requirements

The parking requirements for new uses within the Downtown Specific Plan shall be as outlined in the following table. As discussed in the preceding section, parking requirements include allowances based on the opportunities for shared parking and are intended to promote the “park-once” concept. In addition, parking standards envision that parking can either be provided on site or within common parking facilities through the payment of an “in-lieu” parking fee. This concept will be discussed in detail in the following section.

**Table 16A
Parking Requirements for the Downtown Specific Plan Area**

Uses	Raincross and Justice Center Districts	All Other Districts
General office	1: 250 <u>300</u>	1:250
Medical/Dental office	1: 480 <u>250</u>	4:180 <u>Per Zoning Code</u>
Banks	1:250	4:180 <u>Per Zoning Code</u>
Retail	1:375	Per Zoning Code <u>1:300</u>
Restaurant <u>Eating and Drinking</u>	1: 450 <u>250</u>	Per Zoning Code <u>1:150</u>
Cinema, auditorium	1:8 fixed seats	4:4 fixed seats <u>Per Zoning Code</u>
Motel	4:1 guest room	4:1 guest room
Hotel/ Motel	1: 4 <u>2</u> guest rooms; ancillary uses at 50% of Specific Plan requirement	1:1 guest room; ancillary uses at 50% of Specific Plan requirement
Assembly areas and meeting rooms without fixed seats	1:50	1:30
Schools-Educational and vocational	Per Zoning Code	Per Zoning Code
Residential uses	Per Zoning Code <u>1: bedroom, not to exceed 2 spaces per dwelling unit</u>	1: bedroom, not to exceed 2 spaces per dwelling unit (except for single-family dwellings) <u>Per Zoning Code</u>
Uses not listed	Per Zoning Code	Per Zoning Code

Exemptions: Any new uses within the confines of an existing structure ~~which is a designated historic resource or a contributor to an historic district, as defined in Title 20 of the Riverside Municipal Code,~~ are exempt from providing any additional parking. If an existing building is expanded, additional parking will be required to accommodate the expansion.

Live/Work Units: The parking requirements for live/work units in the downtown shall be based on the requirements of dwelling units only. Since live/work units are primarily for individual entrepreneurs and visitors will be minimal, visitor parking for live/work units should be accommodated by public on-street or off-street parking.

Infill lots: Infill development on legal parcels of record existing on or before October 15, 2021 that are less than 6,000 square feet in net lot area are exempt from providing on-site parking.

Mixed Use Development: The parking requirements for mixed use developments shall be calculated based on the sum of the parking requirements for each individual use. In cases of mixed residential and commercial projects, the residential parking should be separate and secured from the commercial parking spaces.

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16.3 STANDARDS FOR OFF-STREET PARKING AND LOADING

Refer to ~~Chapter 19.74 of~~ the Zoning Code and the Riverside Downtown Design Guidelines for standards for off-street parking and loading. In addition, refer to the recommendations of the Comprehensive Downtown Parking Study by Wilbur Smith Associates.

16.4 PARKING STRATEGIES

In order to meet future parking needs and to achieve the parking goals and policies in Chapter 3: Vision, Goals and Policies, a comprehensive parking management strategy is needed. In addition to recommendations contained in the CDP Study by Wilbur Smith, a coordinated, comprehensive parking strategy should include the following:

1. Rehabilitation of the existing public parking structures to enhance their appearance, visual appeal and ease of use.
2. Improvement of “wayfinding” signage that identifies public parking lots/structures and provides directions to them.
3. In public garages, placement of reserved parking areas for long-stay users on the upper levels and retention of lower levels for short-stay visitor parking.

These recommendations are intended to address previously identified problems of poor visibility and access and poor aesthetics/convenience of existing parking garages, particularly for visitor parking.

4. Periodical review of the parking requirements of the Specific Plan.

As stated earlier, parking requirements are based on the opportunities for shared parking to support the “park once” concept and to avoid the oversupply of parking in the downtown. It is important to periodically review parking requirements to ensure that they continue to be balanced and equitable.

5. Provision of “in-lieu” parking fees for new development as an alternative to providing some or all of the required parking. These fees can be used by the City to finance off-street public parking facilities.

This will provide for greater flexibility for both developers and the City. Rather than requiring new developments to build their own parking on-site, this provides an option of paying an in-lieu fee to the City instead. Funds collected from in-lieu fees would be placed in a parking fund to finance the construction of strategically located parking facilities, which support shared parking and the “park once” concept. Typically, fees are set below the likely cost of constructing all the required parking on-site, but are sufficient to pay the appropriate share of construction. This

would typically be about 50 to 75% of the cost of constructing all required spaces on-site. A simple methodology to determine the “in-lieu” fee will need to be adopted. In order to stay ahead of demand, it is important that at least one new, ~~strategically~~strategically located parking structure be built in the near term.

6. Create a Downtown Parking District to own/manage the public parking supply. This would include the existing supply (lots and garages), as well as new public supply. It would also include all on-street parking.

There would still be independent private lots/garages within the downtown. The Parking District could include only City facilities, although other public facilities (e.g. County) could be included through cooperative agreements. The parking district would manage/operate existing parking, set parking rates, and build new parking. Parking revenues from public facilities, as well as in-lieu parking fees, would flow back to the Downtown Parking District to fund ongoing operation and new construction where possible. This approach allows for a more coordinated and efficient management of parking in the downtown and facilitates shared use parking. The CDP Study includes a detailed discussion of a parking management strategy.

7. Channel parking revenues (on- and off-street) directly back to the Downtown Parking District to fund ongoing operations and new construction where possible.
8. Encourage greater use of transit, bicycling and walking, as well as ridesharing, telecommuting and alternative work schedules, to reduce overall parking demand.
9. Electric vehicle charging spaces should be required within new parking garages (public or private). Those should be placed in a priority/convenient location, along with spaces reserved for alternative fuel vehicles.
10. Develop a parking strategy for the Almond Street District that develops opportunities for public and shared use parking.

Such a parking strategy is needed to address the specific parking needs for the Almond Street District due to on-going and future adaptive re-use of buildings in an area that also includes residential and school uses. While parking structures are not envisioned in this area, shared parking lots may be a viable option.

16.5 BICYCLE PARKING GUIDELINES

All uses that are subject to Design Review are encouraged to provide bicycle parking in conformance with the following guidelines:

16.5.1 Multi-family Residences

Every residential use of 10 or more dwelling units should provide at least one sheltered bicycle parking space for each dwelling unit. Sheltered bicycle parking spaces may be located within a garage, storage shed, basement, utility room or similar area. In those instances in which the residential complex has no garage or other easily accessible storage unit, the bicycle parking spaces may be sheltered from sun and precipitation under an eave, overhang, an independent structure, or similar cover.

16.5.2 Parking Lots

All public and commercial parking structures should provide a minimum of 20 bicycle parking spaces.

16.5.3 Schools

Elementary and middle schools, both private and public, should provide one bicycle parking space for every 10 students and employees. High schools should provide one bicycle parking space for every 5 students and employees. All spaces should be sheltered under an eave, overhang, independent structure or similar cover.

16.5.4 Colleges and Vocational Schools

Colleges and vocational schools should provide one bicycle parking space for every 10 motor vehicles, plus one space for every dormitory unit. Fifty percent of the bicycle parking spaces should be sheltered under an eave, overhang, independent structure, or similar cover.

16.5.5 Commercial Development Greater than One Acre

Commercial development greater than one acre in size should provide one bicycle parking space for every 20 vehicle parking spaces. There should be a minimum of 10 bicycle parking spaces.

16.5.6 Mixed-Use Development

Bicycle parking for mixed-use development should be calculated by using the total number of vehicle parking spaces required for the entire development. A minimum of one bicycle parking space for every 10 vehicle parking spaces is recommended.

16.5.7 Raincross District

Within the Raincross District, bicycle parking for customers and employees should be provided along the street at a rate of at least one space per use. Individual uses may provide their own parking or spaces may be clustered to serve up to six bicycles. Bicycle parking spaces should be located in front of the stores along the street, either on the sidewalks or in specially constructed areas such as pedestrian curb extensions. Inverted “U” style racks are recommended. Bicycle parking shall not interfere with pedestrian passage, leaving a clear area of at least 48 inches between bicycles and other existing and potential obstructions. Customer and employee bicycle parking spaces may or may not be sheltered.

16.5.8 Exemptions

These guidelines do not apply to single family dwelling units.

16.5.9 Location and Design

Bicycle parking should be located conveniently to both the street right-of-way and at least one building entrance. It should be incorporated whenever possible into building design and coordinated with the design of street furniture when it is provided.

16.5.10 Visibility and Security

Bicycle parking should be visible to cyclists from streets, sidewalks or building entrances so that it provides sufficient security.

16.5.11 Options for Storage

Bicycle parking for long-term and employee parking can be met by providing a bicycle storage room, bicycle lockers, racks or other secure storage space inside or outside of the buildings.

16.5.12 Lighting

Bicycle parking should be at least as ~~well-lit~~well-lit as vehicle parking for security.