

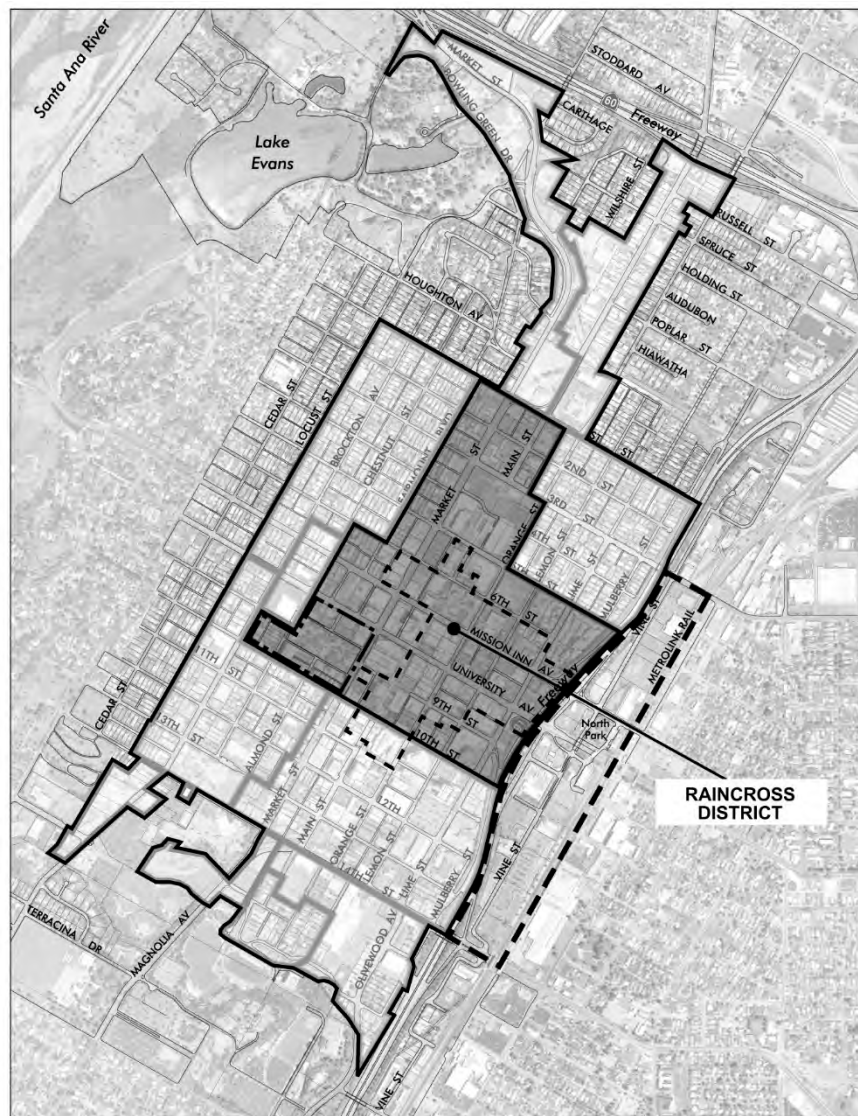
## **CHAPTER 6**

# **RAINCROSS DISTRICT**



This Chapter defines the land uses, development standards and design standards and guidelines for the Raincross District. This Chapter is organized as follows:

- 6.1 Purpose
- 6.2 Permitted Uses
- 6.3 Conditionally Permitted Uses
- 6.4 Prohibited Uses
- 6.5 Development Standards for the Raincross District
- 6.6 Design Standards and Guidelines for the Raincross District
- 6.7 Additional Standards for Live/Work Units in the Raincross District
- 6.8 Additional Standards for Mixed-Use Development in the Raincross District
- 6.9 Additional Standards for Multiple Family Residential Development in the Raincross District



## **6.1 PURPOSE**

The Raincross District is the cultural, historic, and social center of both Riverside and the region beyond. The quality of Downtown Riverside's historic buildings and the relationship between these buildings creates an historic urban fabric unparalleled in the region. The positive image and economic health of Riverside is strongly influenced by this historic character and the protection of that is an essential part of assuring Riverside's economic health and growth into the future. The intent of this specific plan is to create and encourage opportunities for a variety of commercial, residential and entertainment uses, including retail, office, cultural, residential, and visitor serving uses. The Raincross District encourages a concentration of uses that generate activity during both daytime and evening hours. The permitted retail uses in this District are intended to serve the specialty shopping needs of Downtown residents and employees, as well as citywide residents, regional shoppers, and tourists. The Raincross District encourages these uses to occur as both single use buildings and as mixed-use buildings.

The Raincross District is divided into two sub-areas. The center of the District is occupied by the Mission Inn Historic District, which contains Riverside's most important historic buildings. In this sub-area the development standards have been carefully crafted to maintain a scale of development that is compatible with the well-established historic fabric of the district. Outside of the Mission Inn Historic District, the development standards of the District allow greater intensity, while still assuring compatibility of the adjacent historic district and historic residential areas beyond. The development standards for the Raincross District are designed to create a place of daytime, evening and weekend activity by providing a high activity pedestrian environment with a storefront emphasis at the street level. Within the Raincross District, there are numerous local and national historic landmarks that define the district's character, including the Mission Inn, Fox Theater, Stalder Building, Municipal Museum, Unitarian Church, Congregational Church, Municipal Auditorium, Post Office, Loring Building, and Art Museum. Preservation of such structures, along with careful and compatible design of new development is important in maintaining the District's character and unique sense of identity.

The area between Market Street, Fairmount Boulevard, First Street, and Sixth Street is an area of mixed single family, multiple family, vacant property, and commercial uses. Some of the structures in this area have been identified as having historic significance (Downtown Specific Plan/Mile Square West Survey Project). The transition of uses in this area is, therefore, of particular concern. In accordance with Title 20 of the Municipal Code, any development within this area must give careful attention to the preservation of these historic resources, either by retention on site or by relocation to an appropriate area.

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## 6.2 PERMITTED USES

The following uses are permitted in the Raincross District within 165 feet of Fairmount Boulevard between First and Sixth Streets:

- a) Home occupations pursuant to the standards established in the Zoning Code.
- b) Live/work units pursuant to the standards set forth in Section 6.7 of this Chapter.
- c) Mixed-use developments consisting of office and residential uses only and pursuant to the standards set forth in 6.8 of this Chapter.
- d) Multiple-family residential projects having frontage on Main Street (between Third and Tenth Streets) Mission Inn Avenue, or University Avenue are permitted above the first floor only.
- e) Offices, including but not limited to administrative, general, government and professional.

The following uses are permitted in the remainder of the Raincross District:

- a) Art galleries.
- b) Banking establishments.
- ~~e) Business supply retail uses such as, but not limited to, office supplies, equipment rental and repair, photocopy shops, etc.~~
- ~~d)c~~) Catering businesses.
- ~~e)d~~) Education facilities, including vocational schools, with 30,000 square feet or less of floor area.
- ~~f)e~~) Exercise facilities.
- ~~g)f~~) Home occupations pursuant to the standards established in the Zoning Code.
- ~~h)g~~) Libraries.
- ~~i)h~~) Live/work units pursuant to the standards set forth in Section 6.7 of this Chapter.
- ~~j)i~~) Medical and dental offices and laboratories.
- ~~k)j~~) Mixed-use development pursuant to the standards set forth in 6.8 of this Chapter.
- ~~l)k~~) Multiple-family residential dwellings pursuant to the standards set forth in Section 6.9 of this Chapter. Multiple-family residential projects having frontage on Main Street, Mission Inn Avenue, or University Avenue are permitted above the first floor only.
- ~~m)l~~) Museums.
- ~~n)m~~) Offices, including but not limited to administrative, general, government and professional, except on Main Street where they may only be located off-street or behind retail or restaurant uses.
- ~~o)n~~) Parks and open spaces.
- ~~p)o~~) Personal service establishments, such as, but not limited to, barber or beauty shops, tailors, shoe repair, etc.
- ~~q)p~~) Pharmacies.
- ~~r)q~~) Restaurants, including outdoor dining, pursuant to the standards established in the Zoning Code for outdoor dining.
- ~~s)r~~) ~~Specialty Retail uses sales, such as, but not limited to, clothing and jewelry stores, gift shops, novelties, etc.~~

- ~~h)s)~~ Temporary uses, pursuant to the standards established in the Zoning Code and consistent with the purposes of this District.
- ~~u)t)~~ Wireless communication facilities, pursuant to the Zoning Code.
- ~~v)u)~~ Accessory uses, incidental and subordinate to the principal permitted use.
- ~~w)~~ ~~Other uses, not listed above, which are determined by the Planning Director to be similar to those listed above.~~

### **6.3 CONDITIONALLY PERMITTED USES**

The following uses are permitted with a Minor Conditional Use Permit in the Raincross District within 165 feet of Fairmount Boulevard between First and Sixth Streets:

- a) Bed and breakfast inns.
- b) Minor expansions to nonconforming uses with a previously approved Conditional Use Permit.
- c) Vapor recovery systems, pursuant to the Zoning Code.
- d) Wireless communication facilities, pursuant to the Zoning Code.

The following uses are permitted in the remainder of the Raincross District with a Minor Conditional Use Permit:

- a) Arcades.
- ~~b) Assemblies of People – Entertainment, not listed below.~~
- ~~b)c) Assemblies of People – Non-Entertainment, not listed below.~~
- ~~d) Bed and breakfast inns.~~
- ~~e) Brewpub, Breweries, Microbreweries, and Wineries (with off-sale retail and/or on-site tasting),  
Brew-on-premises (with tasting and/or retail sales of prepared beers) pursuant to the standards  
established in the Zoning Code.~~
- ~~a) Entertainment establishments as defined in the Zoning Code.~~
- ~~e)f) Farmer's market.~~
- ~~d)g) Florist with incidental wine sales.~~
- ~~e)h) Indoor multi-tenant sales with 25,000 square feet or less.~~
- ~~f)i) Minor expansions to nonconforming uses with a previously approved Conditional Use Permit.~~
- ~~g)j) On-sale of alcoholic beverages (drinking establishments such as bars, nightclubs, pubs or taverns,  
whose primary business is the sale of alcohol), pursuant to the standards established in the Zoning  
Code.~~
- ~~h)k) Outdoor food preparation.~~
- ~~i)l) Pool hall.~~
- ~~j)m) Vapor recovery systems, pursuant to the Zoning Code.~~
- ~~k)n) Wireless communication facilities, pursuant to the Zoning Code.~~

The following uses are permitted with a Conditional Use Permit in the Raincross District within 165 feet of Fairmount Boulevard between First and Sixth Streets:

- a) Commercial coach as a temporary office.

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- b) Expansion of nonconforming uses.
  - c) Parking structures and facilities with ground floor office, residential, or live/work uses.
  - d) Wireless communication facilities, pursuant to the Zoning Code.

The following uses are permitted in the remainder of the Raincross District with a Conditional Use Permit:

- a) Auditorium.
- ~~a)b)~~ Child day care centers.
- ~~b)c)~~ Cinemas.
- ~~e)~~ Commercial coach as a temporary office.
- d) ~~Clubs and lodges.~~
- e) Convention and conference facilities.
- f) Department stores.
- g) Educational facilities, including vocational schools, with 30,000 square feet or more of floor area.
- h) Expansion of nonconforming uses.
- i) Hotels.
- j) Indoor multi-tenant sales with more than 25,000 square feet.
- k) Liquor stores, pursuant to the standards established in the Zoning Code.
- l) Off-sale of alcoholic beverages (liquor stores, markets, etc.), pursuant to the standards established in the Zoning Code.
- m) Parking structures and facilities with ground floor retail and/or office uses.
- ~~n)~~ Performing arts facilities.
- ~~o)n)~~ ~~Places of worship.~~
- ~~p)o)~~ Public uses not permitted as a matter of right.
- ~~q)p)~~ Public utilities or installations.
- ~~r)q)~~ Recreational facilities.
- ~~s)r)~~ Transit center.
- ~~t)s)~~ Wedding chapels.
- ~~u)t)~~ Wireless communication facilities, pursuant to the Zoning Code.

## 6.4 PROHIBITED USES

The following uses are prohibited in the Raincross District:

- a) Car wash.
- b) Drive-thru establishments.
- c) Motels.
- d) Service stations (gas stations and minor repairs).
- e) Vehicle repair.
- f) Vehicle sales and vehicle parts sales.
- ~~g)~~ ~~Any use not specifically authorized.~~

## **6.5 DEVELOPMENT STANDARDS FOR THE RAINCROSS DISTRICT**

All property in the Raincross District shall be developed in accordance with the following standards. To ensure compatible development with the historic buildings in the Mission Inn Historic District, the maximum allowable height and maximum allowable density in this area is lower than for development in the remainder of the Raincross District. In addition, to protect the adjacent historic residential neighborhood, no vehicular access from or widening of Fairmount Boulevard between First and Sixth Streets should be permitted.



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### 6.5.1 Maximum Floor Area Ratio

#### A. Outside the Mission Inn Historic District:

The maximum floor area ratio (FAR) in the Raincross District, outside of the Mission Inn Historic District, shall be 3.5, except an increased FAR up to, but not exceeding, 4.5 may be permitted for the site amenity features listed in the following table. Amenities may be combined for increased FAR, except that in no case shall the floor area ratio exceed 4.5.

Amenity	Bonus
<b>Landscaped Courtyard or Atrium.</b> A continuous area which is open and accessible to the public, has a minimum area of 600 continuous square feet with a minimum dimension of 20 feet. Such courtyard or atrium shall be landscaped with greenery, statuary, water features, seating, or combination of	Floor area increased four times the square footage contained within the courtyard.
<b>Arcade or Pergola.</b> A covered pedestrian passageway that connects two public streets; a public street and parking area or mall; or a public street and another arcade or pergola. The arcade or pergola shall be unobstructed, have a minimum width of 15 feet and a minimum vertical clearance of 12 feet and shall be situated at street grade. Arcades and Pergola located within the public right-of-way shall require an encroachment permit from the Public Works Department. Where an arcade or pergola is located within the public right-of-way, the minimum width may be reduced to conform with the width of the right-of- way.	Floor area increased three times the square footage contained within the arcade or pergola.
<b>Other Amenities.</b> Additional floor area ratio may be permitted in conjunction with modified amenities or amenities not listed above with the granting of a Conditional Use Permit, provided the proposed amenities specifically support the purpose and intent of the Raincross District and are compatible with surrounding development and design. Specific floor area bonus shall be negotiated and the amenities must be acceptable to the City.	Floor area bonus negotiated in conjunction with the amenities provided and subject to the granting of a Conditional Use Permit.

#### B. Within the Mission Inn Historic District:

The maximum floor area ratio for parcels within the Mission Inn Historic District shall be 3.0, except for the following:

- (1) Additional floor area ratio may be permitted for certain site amenity features set forth in the above table. Amenities may be combined for increased FAR, except that in no case shall the floor area ratio exceed 4.0.
- (2) In addition to the above amenity provision, floor area ratio within the Mission Inn Historic District may be increased up to 4.5 with the approval of a Conditional Use Permit, provided the proposed use specifically supports the purpose and intent of the Raincross District and is compatible with surrounding development and design.

**6.5.2 Maximum Unit Density**

The maximum dwelling unit density shall be 60 units per acre. The maximum unit density may be increased with the approval of a Conditional Use Permit.

**6.5.3 Maximum Height****A. Outside the Mission Inn Historic District:**

The maximum building height in the Raincross District, outside the Mission Inn Historic District, shall be 100 feet, except for the following:

- (1) The maximum building height shall not exceed 50 feet within 100 feet of the Residential District or a residential zone outside the specific plan boundaries. For purposes of this requirement, where the district boundary line runs down the middle of a street or alley or the residentially zoned property is across a street or alley, the 100 feet shall be completely contained on the property proposed for development, not measured from the middle of the street.
- (2) For parcels outside the above referenced 100 feet distance from the Residential District or residential zone, building heights may exceed 100 feet with the granting of a Conditional Use Permit, provided that the maximum FAR is not exceeded.

**B. Within the Mission Inn Historic District:**

The maximum building height in the Raincross District, within the Mission Inn Historic District, shall be 100 feet, provided that anything over 60 feet requires the approval of a Conditional Use Permit and must specifically support the purpose and intent of the Raincross District and be compatible with surrounding development and design.

**6.5.4 Minimum Lot Size**

The minimum lot size for new parcels shall be 10,000 square feet.

**6.5.5 Front Yard Setback**

There shall be no front yard setback; buildings shall be contiguous with the front parcel line (0-foot setback). The front yard setback shall also apply to side and rear yards adjacent to a public street. The following exceptions apply to front yard setbacks:

- (1) A portion of the front building elevation, not to exceed fifty percent of the length of the building frontage, may be setback up to 20 feet to allow for outdoor use, such as outdoor dining, display, public art, entry forecourts, or other amenity appropriate to an urban setback.
- (2) For parcels that have frontage on Mission Inn Avenue between the 91 Freeway and Main Street, the minimum setback shall be 15 feet. The front yard setback should incorporate a combination of “soft” features, such as landscaping, water, etc. and “hard” features, such as pavers, ironwork fencing, etc. No parking is permitted in the front yard setback.

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- (3) Parcels with frontage on Fairmount Boulevard between First and Sixth Streets, shall have a minimum setback along Fairmount Boulevard of 15 feet. This setback should incorporate a combination of “soft” features, such as landscaping, water, etc. and “hard” features, such as pavers, ironwork fencing, etc. No parking is permitted in this setback.

#### **6.5.6 Rear Yard Setback**

No minimum rear yard setback is required, except for the following:

- (1) Where the rear parcel line abuts the Residential District or a residential zone outside the specific plan boundaries, the minimum rear yard setback shall be 15 feet if there is a public alley (distance from building to rear property line or alley easement), and 25 feet where there is no public alley.
- (2) When the project contains a residential component (i.e., multiple family residential use, mixed-use or live/work unit), the building shall be setback a minimum of 15 feet from the rear parcel line. This standard shall not apply to adaptive reuse, or conversion of existing buildings into a residential use.
- (3) Where both of the above situations apply, the greater setback standard shall apply.

#### **6.5.7 Interior Side Yard Setback**

No minimum interior side yard setback is required, except for the following:

- (1) When the project contains a residential component (i.e., multiple family residential use, mixed-use or live/work unit), the portion of the building containing the residential use shall be setback a minimum of 15 feet from the interior side parcel line. This standard shall not apply to adaptive reuse, or conversion of existing buildings into a residential use.

#### **6.5.8 Master Plan for the Riverside School for the Arts**

The proposed Riverside School for the Arts shall be developed as part of a Master Plan approved by the City Council and shall be subject to the development standards and requirements therein.

#### **6.5.9 Multiple-family Residential Projects fronting Main Street, Mission Inn Avenue and University Avenue**

Multiple-family residential projects in the Raincross District that have frontage on Main Street (between Third and Tenth Streets), University Avenue or Mission Inn Avenue must be located above the ground floor or in ground floor rear areas of buildings and shall be combined with a ground floor pedestrian-oriented retail or restaurant use, as permitted in this District, to create an active retail edge on these streets. Such projects are subject to standards set forth in Section 6.8 for mixed-use development.

#### **6.5.10 Parking**

Refer to Chapter 16 for off-street parking requirements and standards.

## **6.6 DESIGN STANDARDS AND GUIDELINES FOR THE RAINCROSS DISTRICT**

### **6.6.1 District Character Defining Statement**

The Raincross District is the cultural, entertainment, and retail center of Riverside and the region beyond. Its significant, signature buildings include the Fox Theater, Stalder Building, Mission Inn, Municipal Museum, Unitarian Church, Congregational Church, Municipal Auditorium, Post Office, Loring Building, and Art Museum. Historic and cultural resource sensitivity are the key concepts in this district. Buildings that contribute to the historic character of this district should be preserved or restored to an authentic historic design. New construction should be in scale and architecturally harmonious with nearby historic buildings. The above listed signature buildings should be used for inspiration regarding design, form, detailing and site layout.

The design standards and guidelines for the Raincross District are intended to enhance both these signature buildings and their setting which together contribute to the character of a cohesive downtown. In addition, the design standards and guidelines for the Raincross District are intended to create a vibrant, pedestrian friendly downtown by encouraging pedestrian orientation to the storefronts, human scaled spaces, and pedestrian amenities.



*Fox Theater, 3801 Mission Inn Avenue*



*Loring Building, 3673 Main Street*



*Municipal Museum, 3720 Orange Street*



*Mission Inn, 3645 Mission Inn Avenue*



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A good example of a preservation sensitive approach to fulfilling the needs for office and commercial lease space in the District's Mission Inn Historic District can be found in the Stalder Building and Imperial Hardware Building Historic Resources Survey, by architect Wayne Donaldson, completed in January 2001. Mr. Donaldson was hired by the City to investigate development opportunities on the sites currently occupied by the Stalder Building (situated on the east corner of Mission Inn Avenue and Market Street) and the Imperial Hardware Building (situated on the east side of the Downtown Mall, between Mission Inn and University Avenues). A developer interested in creating a "Lifestyle Center" in this area suggested these buildings be removed and replaced with new structures.

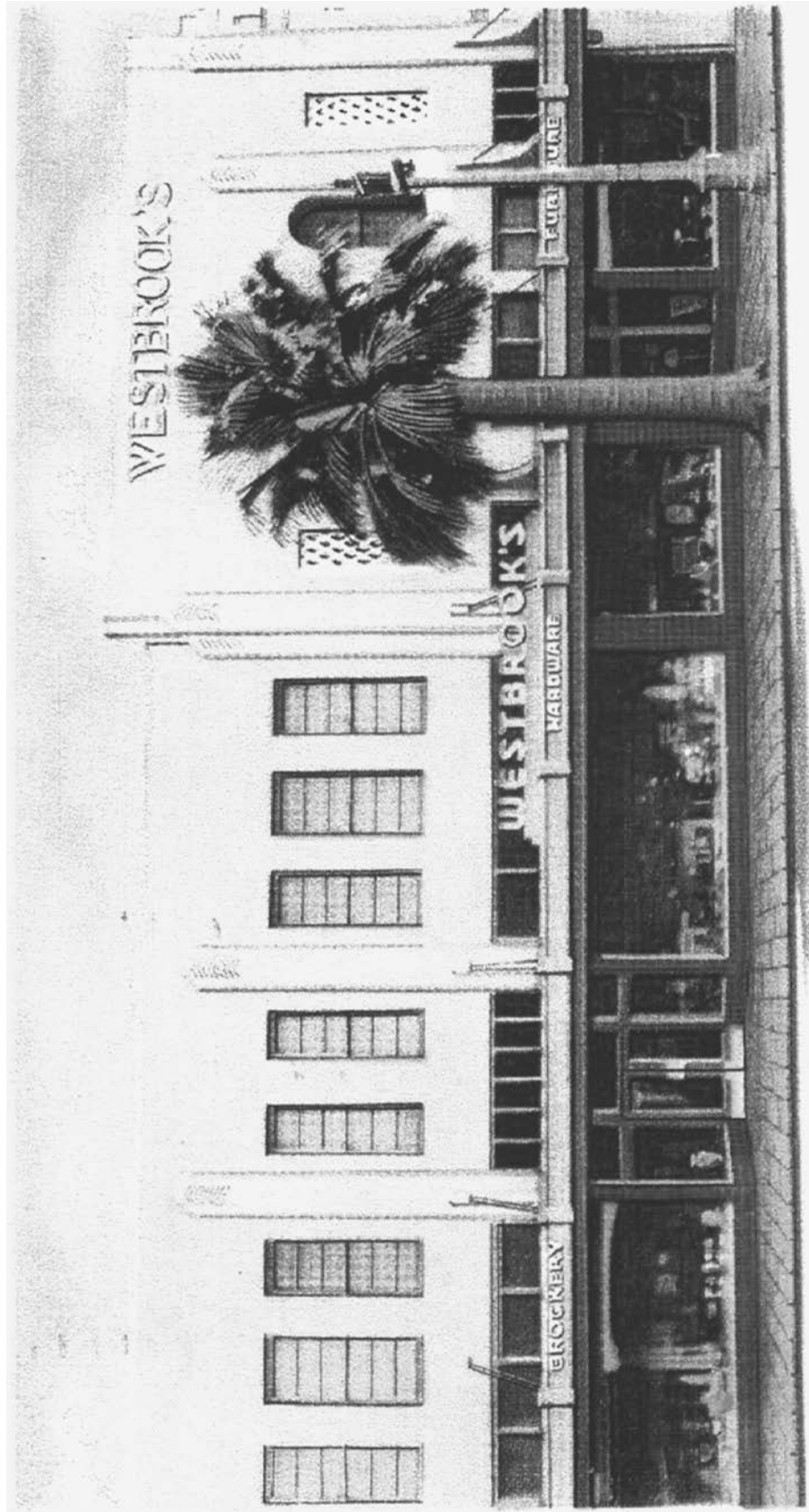
The Donaldson report investigates the historic significance of the buildings, their structural integrity, and the potential for a variety of development options. The report demonstrates the pros and cons of everything from full preservation to adaptive reuse options that would preserve various significant historic aspects of the buildings. It also documents the presence of historic structural members hidden inside the Stalder Building and an intact Art Deco façade covered by a 1960's era metal false front on the Imperial Hardware Building. The report shows how both the Stalder and the Imperial Hardware Buildings could be put to greater economic use with additions that would allow the original historic fabric of the buildings to be preserved and restored. These ideas allow both sites to be developed to the maximum floor area ratio (FAR) permitted by the Specific Plan, while preserving the essence of the historic buildings. As development opportunities are explored that affect older buildings in the Raincross District, consideration of various preservation alternatives should be undertaken in the fashion of the Donaldson report.



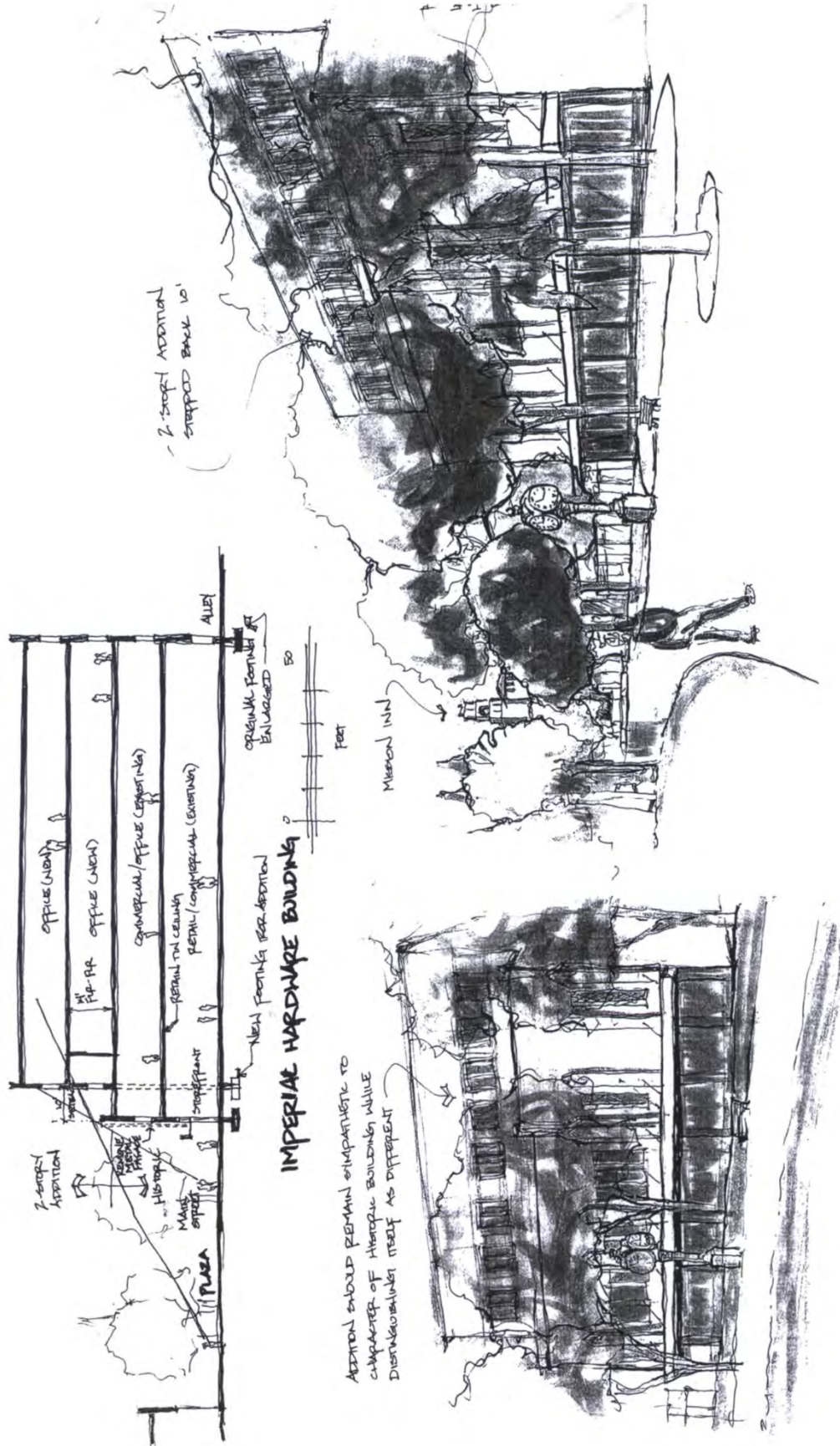
*Stalder Building*







*Imperial Hardware - Westbrook's Hardware Store, ca. 1935, Postcard Courtesy of Riverside Municipal Museum  
Source: Stalder Building and Imperial Hardware Building, Historic Resources Survey, Wayne Donaldson, January 25, 2001*



Imperial Hardware - Two additional office levels  
Source: Stalder Building and Imperial Hardware Building, Historic Resources Survey, Wayne Donaldson, January 25, 2001





**6.6.2 Site Planning****Building Orientation**

- (1) Buildings should have a strong street presence, with public entrances and activity areas oriented toward the street.
- (2) There is a pattern of first floors elevated above sidewalk grade along Mission Inn Avenue between the 91 Freeway and Orange Street. This pattern should be maintained. Building entries should be raised up to three feet from the adjoining street level.

**Setbacks**

- (1) Commercial buildings should generally have a direct interface with public sidewalks with no intervening setback.
- (2) Buildings along Mission Inn Avenue between the 91 Freeway and Orange Street and between Fairmount Boulevard and Chestnut Street should be setback a distance that is compatible with other nearby buildings.
- (3) These setback areas should generally include a combination of “soft” features (landscaping, water, etc.) and “hard” features (pavers, steps, patios, arcades, porches, etc.). For parcels with frontage on Mission Inn Avenue; architectural elements such as stairs or steps, and urban amenities such as benches, water fountains, and public art are encouraged.

**Vehicular Access and Parking**

- (1) Parking spaces should not have direct frontage on a street. Access points should be limited to existing alleys wherever possible.
- (2) When on-site parking is provided, parking should be consolidated on the site. In no case, should the building be surrounded on all sides with parking.
- (3) No vehicular access to or from Fairmount Boulevard should be permitted between First and Sixth Streets within this District.

**Pedestrian Access**

- (1) Primary access to buildings should be from the street or pedestrian walkways, not parking areas.
- (2) Walkways should be provided to link parking areas with the street wherever feasible.

**Interface between Non-residential and Residential Uses**

In several portions of the Specific Plan area, non-residential uses abut residential uses. This condition can specially be observed in the Raincross District, along Fairmount Boulevard between First and Sixth Streets. Issues of privacy, safety, and noise are addressed in these following standards:

- (1) To provide privacy for adjacent residential properties, taller elements of the building should be set away from those properties. In addition, at residential edges, commercial buildings should maintain low profiles and building heights should be stepped down to the height of adjacent residential zones, utilizing architectural elements such as gables or hip roofs to reduce building mass.

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- (2) When there is no intervening alley or street, appropriate landscape screening shall be provided at the shared property line. Excepting trees, this screening shall not be less than six feet or exceed eight feet in height.
  - (3) Eighty percent of the vertical plane at the property line to a height of six feet shall be opaque.
  - (4) Screening may consist of one (or more) of the following:
    - “Vertical” trees closely spaced
    - “Green” (vine-covered) solid or fenced walls
    - Hedges (minimum height of six feet)
  - (5) The criteria for selecting plant materials, as established in section 15.3.6 shall be followed.
  - (6) Noise or odor generating activities in general, and loading areas, trash and storage areas, and rooftop equipment in particular, should be located as far as possible from adjacent residential uses and shall not be located next to residential properties without fully mitigating their negative effects.
  - (7) Non-residential buildings should be sited so as to avoid significant shading of adjacent residences and compromising residents’ privacy.
  - (8) Windows in non-residential buildings should be oriented to avoid a direct line of sight into adjacent residential buildings or property.
  - (9) Whenever adjacent residential and commercial uses can mutually benefit from connection rather than separation, appropriate connective elements such as walkways, common landscaped areas, building orientation, gates, and/or unfenced property lines should be employed.

#### **Site Furniture**

- (1) Pedestrian amenities are encouraged, including benches, landscaped gathering areas, trash receptacles, etc. Design of improvements should be traditional and related to the signature buildings. Particular attention should be paid to creating shade in the Raincross District as well as all the other districts.

#### **Courtyards and Passages**

- (1) When placed in an appropriate location, between two elements of a building or buildings, a courtyard can provide a visually relaxing pedestrian environment, or a secluded retreat from noise and traffic. Courtyards play an important role in providing spaces for solace and respite in Downtown. Courtyards and pedestrian passages are encouraged to facilitate pedestrian circulation and to provide resting and gathering places. Courtyards should be prominently placed on the site to be seen from the street and to enhance the public environment. They should be placed to terminate vistas, to anchor street corners and along street edges.
- (2) All courtyards should be provided with amenities such as shade trees, seating areas, water fountains, accent planting, and public art. If space permits, elements such as gazebos, arcades, or pergolas should be incorporated in the courtyard design.
- (3) Design of improvements should be traditional and related to the signature buildings.

**6.6.3 Architecture****Style**

- (1) Existing buildings should be restored/maintained in a historic style that reflects the actual, historic appearance of the building at its period of historic significance.
- (2) The historic fabric in Downtown Riverside is interspersed with “contextual” buildings - buildings that are not historic but contribute to the district character as one traverses the district. Similarly, new buildings should not necessarily be stylistically “historic”, but should be compatible with their historic neighbors in terms of massing, modulation, height, and setbacks. New buildings should be contemporary interpretations using the signature buildings as a source of design inspiration.

**Scale**

- (1) Buildings and improvements should be at a pedestrian scale. To maintain a sense of pedestrian scale, larger buildings should be broken into storefront bays about 25 feet wide.
- (2) The size and mass of a new building should blend with the surrounding district.

**Detailing**

- (1) Detailing of existing buildings should be a restoration or replication of historic detailing during the building’s period of historic significance.

**Roof Design**

- (1) Roof design should reflect/complement significant buildings in the area.

**Colors and Materials**

- (1) Muted earthtones and traditional materials should prevail, with brighter colors limited to trim areas. The Mission Inn is a good example of this type of treatment.

**6.6.4 Landscaping****Plant Types**

- (1) Landscaping should be compatible with historic plantings and consist of types suitable for the climate and the exposure in which they are to be planted.

**Scale**

- (1) Pedestrian scale plantings should prevail, with larger plantings used as accents.

**Relationship to Development**

- (1) Plantings should be used to complement and accent the architecture. They should not be of such a scale and density as to obscure or overwhelm the architecture.

**Hardscape**

- (1) Paved areas should make significant use of traditional concrete scoring and pavers.

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### **6.6.5 Signs**

#### **Style**

- (1) Signs should be low-key and complementary to the architecture. Eating and entertainment uses may make greater use of color and innovative design. Historic forms and types should predominate.

#### **Scale**

- (1) Signing should be an accent; the overall effect should be low key and proportional to the building.

#### **Type**

- (1) A variety of forms is acceptable, including painted wood, painted metal, signs painted directly on the building, and traditional neon. Interior illuminated signs, plastic, canister signs, channel letter signs, and other more modern forms of signs are generally not appropriate.

### **6.6.6 Additional Design Standards and Guidelines**

Refer to Chapter 15 for general design standards and guidelines that apply to the entire Downtown Specific Plan area.

## **6.7 ADDITIONAL STANDARDS FOR LIVE/WORK UNITS IN THE RAINCROSS DISTRICT**

### **6.7.1 Applicability**

The provisions of this section apply to live/work units, as defined in Chapter 18: Definitions. These standards are in addition to the development standards set forth in Section 6.5 and the applicable design guidelines. Refer to Chapter 16 for parking standards and Chapter 17 for sign standards. The type of work permitted in a live/work unit shall be consistent with, or similar in nature to, the permitted uses in the Raincross District, as defined in Section 6.2, and home occupations as listed in the Zoning Code.

### **6.7.2 Purpose**

The intent of this section is to provide for and make feasible the reuse of existing commercial or residential structures to accommodate live/work opportunities, as well as provide opportunities for the new development of buildings specifically designed and constructed to provide live/work units. Live/work units may also occupy a portion of a building designed for mixed-use development.

**6.7.3 Floor Area Requirements**

The minimum floor area of a live/work unit shall be 750 square feet. For live/work units that have frontage on Mission Inn Avenue, Main Street or University Avenue, the first 25 feet of floor area depth at the street-level frontage shall be devoted to pedestrian-oriented commercial retail activity. Live/work units with frontage on public streets in all other areas of the Raincross District, or on the upper levels of a structure shall not be subject to this requirement.

**6.7.4 Access to Units**

Access to individual units shall be from common access areas, corridors, or hallways.

**6.7.5 Internal Layout**

All living space within the live/work unit shall be contiguous with and an integral part of the working space, with direct internal access between the two areas.

**6.7.6 Street Frontage Treatment**

Each live/work quarters fronting Mission Inn Avenue, Main Street or University Avenue shall have a pedestrian-oriented frontage that publicly displays the interior of the nonresidential areas of the structure. Live/work units with frontage on public streets in all other areas of the Raincross District, and on the upper levels of a structure shall not be subject to this requirement.

**6.7.7 Occupancy and Employees**

At least one of the full-time workers of the live/work unit shall reside in the unit. The residential area shall not be rented separately from the working space. The business activity occupying the live/work unit may utilize employees in addition to residents as necessary.

**6.7.8 Retail Sales**

Retail space may be integrated with working space.

**6.7.9 Business License**

A business license shall be obtained in compliance with the Municipal Code for business activities conducted within the live/work unit.

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## **6.8 ADDITIONAL STANDARDS FOR MIXED-USE DEVELOPMENT IN THE RAINCROSS DISTRICT**

### **6.8.1 Applicability**

The provisions of this section apply to mixed-use development, as defined in Chapter 18: Definitions, in the Raincross District. These standards are in addition to the development standards set forth in Section 6.5 and the applicable design guidelines. Refer to Chapter 16 for parking standards and Chapter 17 for sign standards.

### **6.8.2 Purpose**

The intent of this section is to strengthen the interaction between residential, commercial and employment uses so as to facilitate a more efficient use of transportation systems, to encourage the conservation of land resources and create a vital urban area that is a place of daytime, evening and weekend activity.

### **6.8.3 Land Use Requirements for Mixed-use Development**

Mixed-use development integrates compatible office or commercial uses with residential uses within the same building or structure. Mixed-use development in the Raincross District should generally promote retail uses at the street level, and shall have the following use requirements:

1. Ground Floor or Street Level
  - (a) Retail uses - The ground floor or street level shall be devoted to pedestrian-oriented ~~specialty~~ retail, restaurant, or similar type of use, except within 165 feet of Fairmount Boulevard between First and Sixth Street where retail uses are prohibited.
  - (b) Office uses - General and professional office uses shall be allowed on the ground floor, except on Main Street where they may only be located off-street or behind retail or restaurant uses to create an active retail edge.
  - (c) Residential or lodging uses - Mixed-use projects that have frontage on Main Street, Mission Inn Avenue, or University Avenue may have residential or lodging uses on the ground floor only when located off-street or behind retail uses. Residential or lodging uses shall be permitted on the ground floor for all other areas of the Raincross District. A common entrance to the residential portion of the mixed-use project may be located adjacent to the non-residential front, ground floor use.
  - (d) Live/work uses - Live/work units shall be permitted on the ground floor, subject to the standards for live/work units set forth in Section 6.7. For mixed-use projects that have frontage on Main Street, Mission Inn Avenue or University Avenue, the first 25 feet of floor area depth at the street level frontage shall be devoted to pedestrian-oriented commercial retail activity. Live/work unit on the ground floor of all other mixed-use projects shall not be subject to this requirement.

2. Upper Levels

- (a) The upper levels may contain retail (except within 165 feet of Fairmount Boulevard between First and Sixth Streets where commercial uses are prohibited), office or lodging uses, however, at least one floor of the upper levels must be dedicated to residential or live/work uses.

**6.8.4 Lot Area and Width**

1. The minimum lot area for any new mixed-use development shall be 10,000 square feet.
2. There shall be no minimum lot area or width requirements for the conversion of existing buildings to mixed-use developments.

**6.8.5 Development Density**

Pursuant to Section 6.5 of this Chapter, the maximum FAR and maximum unit density may be increased with the approval of a Conditional Use Permit, provided the mixed-use project specifically supports the purpose and intent of the Raincross District and is compatible with surrounding uses. The request for increased development intensity shall be reviewed as part of the Conditional Use Permit application for the mixed-use development project.

**6.8.6 Maximum Height**

Pursuant to Section 6.5 of this Chapter, the maximum building height may be increased with the approval of a Conditional Use Permit, provided the mixed-use project specifically supports the purpose and intent of the Raincross District and is compatible with surrounding uses. The request for increased building height shall be reviewed as part of the Conditional Use Permit application for the mixed-use development project.

**6.8.7 Yard Setbacks**

1. The setback requirements set forth in Section 6.5 of this Chapter shall apply to mixed-use development.

**6.8.8 Open Space**

The following regulations shall determine the amount of required private and common open space:

1. New Projects:
  - (a) Private Usable Open Space: At least 50% of the dwelling units in a project shall provide private usable open space, as defined in the Zoning Code, of a minimum of 50 square feet. All dwelling units in a project are encouraged to include private usable open space. A rectangle inscribed within each private usable open space shall have no dimension less than five feet. At least one exterior side shall be open above the level of railing or fencing. Balcony/railing enclosures shall not be see-through.



- 
- (b) Common Usable Open Space: At least 50 square feet of common usable open space, as defined in the Zoning Code, shall be provided per dwelling unit. Common usable open space may be divided into more than one area, however, each area shall be a minimum of 450 square feet and a rectangle inscribed within each shall have no dimension less than 20 feet. All required common open space shall be suitably improved for its intended purposes and all lawn and landscaped areas shall be provided with a permanent irrigation system to maintain such areas. The common open space may include courtyards, terraces and rooftops.

2. Conversions of existing buildings:

- (a) There shall be no minimum open space standards for conversions of existing buildings, however, every effort shall be made to achieve open space in all of the above categories to the extent feasible for the building being converted.

~~6.8.9 Conditional Use Permit~~

~~A Conditional Use Permit shall be required for any mixed-use development project. Application and fee information may be obtained at the Planning Department.~~

## 6.9 ADDITIONAL STANDARDS FOR MULTIFAMILY RESIDENTIAL DEVELOPMENT IN THE RAINCROSS DISTRICT

### 6.9.1 Applicability

The provisions of this section apply to multifamily residential development in the Raincross District.

### 6.9.2 Purpose

Historically, town centers included a mix of shops, offices, and restaurants at the street level and residences above. This pattern created a rich mix of uses and allowed urban activity to carry on well into the nighttime. In the 20th century, downtowns were increasingly zoned for one activity only, namely business, and were shutdown after six in the evening. In recent years, cities have once again begun to encourage residential uses within their downtowns and thus tap into the potential of increased economic activity by extending the usability of downtown resources into the nighttime. Residential uses are encouraged in the Raincross District both as a part of mixed use and multifamily residential developments.

Any multifamily structure shall conform to the standards and guidelines contained in section 6.5 and 6.6. Additional standards and guidelines are included in this section.



*Examples of urban multi-family residential projects*

### **6.9.3 Site Planning**

#### **Building Orientation**

- (1) Buildings should have a strong street presence, with public entrances oriented towards the street.
- (2) Privacy between units should be maintained by locating balconies, porches, windows, etc., away from similar existing spaces in the adjacent units.

#### **Setbacks**

- (1) While no setback is required in all of Raincross District, except on parts of Mission Inn Avenue, the lowermost floor may be recessed to create space for landscape screening in multifamily residential developments.
- (2) On Mission Inn Avenue, the setback area should be used for planting landscape screens to protect the privacy of the ground floor units.

#### **Vehicular Access and Parking**

- (1) Parking should be provided on site, located to the rear of the parcel wherever possible, in on-grade or underground structures or landscaped lots. Such parking areas should be fully screened from the street and security controls are permitted. Access should be limited to the minimum number of drives required to serve the required parking spaces. Except for town home projects with attached garages, all multi-family parking should be in efficient, multiple stall configurations.
- (2) If tuck-under parking is provided, the first floor of the residential units shall not occur more than four feet above the finished grade level. Parking may need to be depressed and occur behind the living spaces. Finished grades of the front entrances may be raised by up to four feet to accommodate this arrangement. These parking areas shall not be visible from the street.

#### **Pedestrian Access**

- (1) Pedestrian access to the first floor units should be via traditional residential front doors.

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### **Open Space**

The following is required for private and common open space for multifamily development:

- (1) Private Usable Open Space: Each dwelling unit shall have a minimum private usable open space as defined in the Zoning Code of 50 square feet. A rectangle inscribed within each private usable open space shall have no dimension less than five feet. At least one exterior side shall be open above the level of railing or fencing. Private open space features include: fenced yard/patio areas, balconies and roof gardens.
- (2) Common Usable Open Space: Inclusive of required setback yards, a minimum of 20 percent of the site area shall be designated and permanently reserved as usable common open space in multiple family developments with greater than 10 dwelling units. All required common open space shall be suitably improved for its intended purposes and all lawn and landscaped areas shall be provided with a permanent irrigation system to maintain such areas. Common open space areas include courtyards, terraces, rooftops, landscaping, picnic/barbecue areas, pools/spas, tennis/sport courts, clubhouse, totlots/playgrounds. Parking areas are not to be included in the calculation of open space.

### **Site Furniture**

- (1) Outdoor pedestrian amenities should be provided in the 'Usable Open Space' areas required as noted in the section above.

### **Courtyards and Passages**

- (1) 'Common Usable Open Space' areas should be provided as noted in the section on Open Space above.

## **6.9.4 Architecture**

### **Style**

- (1) Multiple family housing is unique in the sense that while an individual, private use, it is also a part of the urban fabric. Residents affect, and are affected by, street activity and provide "eyes on the street". The residential character of the individual units should be protected while conforming to the urban feel of the Raincross District.

### **Scale**

- (1) Individual units should be articulated to diminish the massing of large structures and be compatible with the scale of surrounding development.
- (2) The mass and roof forms of buildings should be varied. In addition to porches, stoops and other entry elements such as bay windows, balconies and trellises are encouraged.
- (3) The street floor building level should be raised between two and four feet to protect the privacy of ground floor units.
- (4) Facades of multifamily buildings should be divided into shorter modules a maximum of 30 feet in width, to reflect the volumes of individual units within the building. This objective can be achieved with varied setbacks, vertical modulation, texture changes on the facade, porches and balconies.

### **6.9.5 Landscaping**

#### **Plant Types**

- (1) Plantings of shrubs and flowering plants to add variety to the setback areas are encouraged.
- (2) Pathways and pergolas and trellises that are in character with the architectural style of development to add shade and interest are encouraged.

#### **Relationship to Development**

- (1) Common areas provided for the residents should be landscaped with planted areas, potted plants, seating, outdoor barbecue areas, pools and spas, etc.

### **6.9.6 Signage**

#### **Style**

- (1) Traditional designs that reflect the building architecture are encouraged.
- (2) The signage for multifamily uses in this district should be discreet and subdued.

## **CHAPTER 7**

# **JUSTICE CENTER DISTRICT**

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- 7.1 Purpose
- 7.2 Permitted Uses
- 7.3 Conditionally Permitted Uses
- 7.4 Prohibited Uses
- 7.5 Development Standards for the Justice Center District
- 7.6 Design Standards and Guidelines for the Justice Center District



## **7.1 PURPOSE**

The intent of the Justice Center District is to provide a center for the existing and future expansion of major judicial uses, including federal, appellate, state and county courts. This District allows for a concentration of office uses and ~~business supply~~ retail uses to maintain Downtown as the employment center for the City of Riverside. Development intensity in the Justice Center is the highest in the Downtown and is intended to encourage medium to high density office uses to support and complement the retail, cultural, entertainment and residential uses of the nearby Raincross District. The scale and grandeur of the Beaux-Arts style Riverside County Courthouse, located on Main Street, provides the opportunity to emphasize the judicial nature of this District by opening and reinforcing the view to this important architectural and historical structure from Market Street.

## **7.2 PERMITTED USES**

The following uses are permitted in the Justice Center District:

- a) Banking establishments.
- ~~b) Business supply retail uses such as, but not limited to, office supplies, equipment rental and repair, photocopy shops, etc.~~
- ~~e)b~~) Courts.
- ~~d)c~~) Offices, including but not limited to administrative, general, government and professional.
- ~~e)d~~) Parks and open spaces.
- ~~f)e~~) Personal service establishments, such as, but not limited to, barber or beauty shops, tailors, and shoe repair, ~~etc.~~
- f) Restaurants, including outdoor dining, that are 1,500 square feet or less in size, or within lease space 1,500 square feet or less in size. Outdoor dining is subject to the standards established in the Zoning Code for outdoor dining.
- g) Retail sales.
- ~~g)h~~) Temporary uses, pursuant to the standards established in the Zoning Code and consistent with the purposes of this District.
- ~~h)i~~) Wireless communication facilities, pursuant to the Zoning Code.
- ~~i)j~~) Accessory uses, incidental and subordinate to the principal permitted use.
- ~~j)k~~) Other uses, not listed above, which are determined by the Planning Director to be similar to those listed above.

## **7.3 CONDITIONALLY PERMITTED USES**

The following uses are permitted in the Justice Center District with a Minor Conditional Use Permit:

- a) Brewpubs, Breweries, Microbreweries, Wineries (with off-sale retail and/or on-site tasting), Brew-on-premises (with tasting and/or retail sales of prepared beers) pursuant to the standards established in the Zoning Code.
- ~~a)b~~) Farmer's market.
- ~~b)c~~) Minor expansions to nonconforming uses with a previously approved Conditional Use Permit.
- ~~e)d~~) On-sale of alcoholic beverages, pursuant to the standards established in the Zoning Code.
- ~~d)e~~) Outdoor food preparation.



- 
- ~~e)f)~~ Restaurants, including outdoor dining, that are more than 1,500 square feet in size, or within lease space greater than 1,500 square feet in size. Outdoor dining is subject to the standards established in the Zoning Code for outdoor dining.
  - ~~f)g)~~ Vapor recovery systems, pursuant to the Zoning Code.
  - ~~g)h)~~ Wireless communication facilities, pursuant to the Zoning Code.

The following uses are permitted in the Justice Center District with a Conditional Use Permit:

- a) Assemblies of People – Entertainment.
- b) Assemblies of People – Non-Entertainment.
- ~~h)c)~~ Catering businesses.
- ~~i)d)~~ Child day care centers.
- ~~j)e)~~ Educational facilities, including vocational schools.
- ~~k)f)~~ Exercise facilities.
- ~~h)g)~~ Expansion of nonconforming uses.
- ~~m)h)~~ Parking structures and facilities with ground floor commercial uses.
- ~~n)i)~~ Public uses not permitted as a matter of right.
- ~~o)j)~~ Public utilities or installations.
- ~~p)k)~~ Service stations (gas stations with minor repairs and/or food marts with or without the off-sale of beer and wine) on parcels adjacent to a freeway on or off ramp.
- l) Wireless communication facilities, pursuant to the Zoning Code.

## 7.4 PROHIBITED USES

The following uses are prohibited in the Justice Center District:

- a) Car wash.
- b) Drive-thru establishments.
- c) Motels.
- d) Hotels.
- e) Vehicle repair.
- f) Vehicle sales and vehicle parts sales.
- ~~g) Any use not specifically authorized.~~

**7.5 DEVELOPMENT STANDARDS FOR THE JUSTICE CENTER DISTRICT**

All property in the Justice Center District shall be developed in accordance with the following standards. To ensure compatible development with the historic buildings in the Mission Inn Historic District, the maximum allowable height and maximum allowable density in this area is lower than for development in the remainder of the Justice Center.

**7.5.1 Maximum Floor Area Ratio****A. Outside the Mission Inn Historic District:**

The maximum floor area ratio (FAR) in the Justice Center District, outside of the Mission Inn Historic District shall be 5.0, except an increased FAR up to, but not exceeding, 6.0 may be permitted for the site amenity features listed in the following table. Amenities may be combined for increased FAR, except that in no case shall the floor area ratio exceed 6.0.

<b>Amenity</b>	<b>Bonus</b>
<b>Landscaped Courtyard or Atrium.</b> A continuous area which is open and is accessible to the public, has a minimum area of 600 continuous square feet with a minimum dimension of 20 feet. Such courtyard or atrium shall be landscaped with greenery, statuary, water features, seating, or combination of the four.	Floor area increased four times the square footage contained within the courtyard.
<b>Arcade or Pergola.</b> A covered pedestrian passageway that connects two public streets; a public street and parking area or mall; or a public street and another arcade or pergola. The arcade or pergola shall be unobstructed, have a minimum width of 15 feet and a minimum vertical clearance of 12 feet and shall be situated at street grade. Arcades and Pergola located within the public right-of-way shall require an encroachment permit from the Public Works Department. Where an arcade or pergola is located within the public right-of-way, the minimum width may be reduced to conform with the width of the right-of- way.	Floor area increased three times the square footage contained within the arcade or pergola.
<b>Other Amenities.</b> Additional floor area ratio may be permitted in conjunction with modified amenities or amenities not listed above with the granting of a Conditional Use Permit, provided the proposed amenities specifically support the purpose and intent of the Justice Center District and are compatible with surrounding development and design. Specific floor area bonus shall be negotiated and the amenities must be acceptable to the City.	Floor area bonus negotiated in conjunction with the amenities provided and subject to the granting of a Conditional Use Permit.

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**B. Within the Mission Inn Historic District:**

The maximum floor area ratio for parcels within the Mission Inn Historic District shall be 3.0, except for the following:

- (1) Additional floor area ratio may be permitted for certain site amenity features set forth in the above table. Amenities may be combined for increased FAR, except that in no case shall the floor area ratio exceed 4.0.
- (2) In addition to the above amenity provision, floor area ratio may be increased up to 4.5 with the approval of a Conditional Use Permit, provided the proposed use specifically supports the purpose and intent of the Justice Center District and is compatible with surrounding development and design.

**7.5.2 Minimum Height**

The minimum building height shall be 45 feet for at least 50% of the building footprint. Parcels in the Mission Inn Historic District are not subject to this requirement.

**7.5.3 Maximum Height**

**A. Outside the Mission Inn Historic District:**

The maximum building height in the Justice Center, outside the Mission Inn Historic District, shall be 140 feet, except for the following:

- (1) For parcels west of Market Street, the maximum building height shall be 60 feet.
- (2) For parcels that have frontage on the east side of Market Street, the maximum building height shall be 60 feet to a depth of 50 feet from the front property line. The building height may step up to the maximum allowable building height at a point 50 feet from the front property line.
- (3) For parcels outside the 50 foot setback east of Market Street defined above, building heights may exceed 140 feet with the granting of a Conditional Use Permit, provided that the maximum FAR is not exceeded.

**B. Within the Mission Inn Historic District:**

The maximum building height in the Justice Center, within the Mission Inn Historic District, shall be 60 feet, except for the following:

- (1) The maximum building height may be increased to a maximum height of 100 feet with the approval of a Conditional Use Permit, provided the proposed use specifically supports the purpose and intent of the Justice Center District and is compatible with surrounding development and design.

**7.5.4 Minimum Lot Size**

The minimum lot size for new development or for the creation of new parcels shall be 20,000 square feet.

**7.5.5 Front Yard Setback**

The minimum front yard setback shall be 10 feet; the maximum front yard setback shall be 30 feet. No parking is permitted in the front yard setback area. The front yard setback shall also apply to side and rear yards adjacent to a public street. The following exception applies to front yard setbacks:

- (1) For parcels that have frontage on Market Street, the minimum front yard setback shall be 15 feet; the maximum front yard setback shall be 30 feet.

**7.5.6 Rear Yard Setback**

No minimum rear yard setback is required, except for the following:

- (1) Where the rear parcel line abuts the Almond Street District, the minimum rear yard setback shall be 15 feet if there is a public alley (distance from building to rear property line or alley easement), and 25 feet where there is no public alley.

**7.5.7 Interior Side Yard Setback**

No minimum interior side yard setback is required, except where the side parcel line abuts the Almond Street District, the minimum interior side yard setback shall be 15 feet.

**7.5.8 Parking**

Refer to Chapter 16 for off-street parking requirements and standards.

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## 7.6 DESIGN STANDARDS AND GUIDELINES FOR THE JUSTICE CENTER DISTRICT

### 7.6.1 District Character Defining Statement

The Justice Center is the legal and office center of Riverside and the surrounding region. Restaurant and retail uses are intended to be limited to those that are incidental to, and for the convenience of, the office and legal uses. The significant, signature buildings in this district include the Superior Court Building, Hall of Justice, Family Law Court, Bankruptcy Court, U.S. District Court, and the Catholic Church. This is primarily a district for contemporary buildings with historic references in detailing, massing, fenestration, and shapes. The district's historic buildings should be respected and complemented in any adjacent architecture. While taller buildings are anticipated in this district, respecting pedestrian scale is important. One example of how this can be accomplished is by the stepping of building heights from lower at the street to higher beyond. The above listed signature buildings should be used for inspiration regarding design, form, detailing, and site layout.



*U.S. District Court of Appeals (12<sup>th</sup> and Lime Streets)*



*Bankruptcy Court (12<sup>th</sup> and Lime Streets)*



*Riverside County Courthouse (Main and 10<sup>th</sup> Streets)*



*Family Law Court (Main and 11<sup>th</sup> Streets)*

## 7.6.2 Site Planning

### Building Orientation

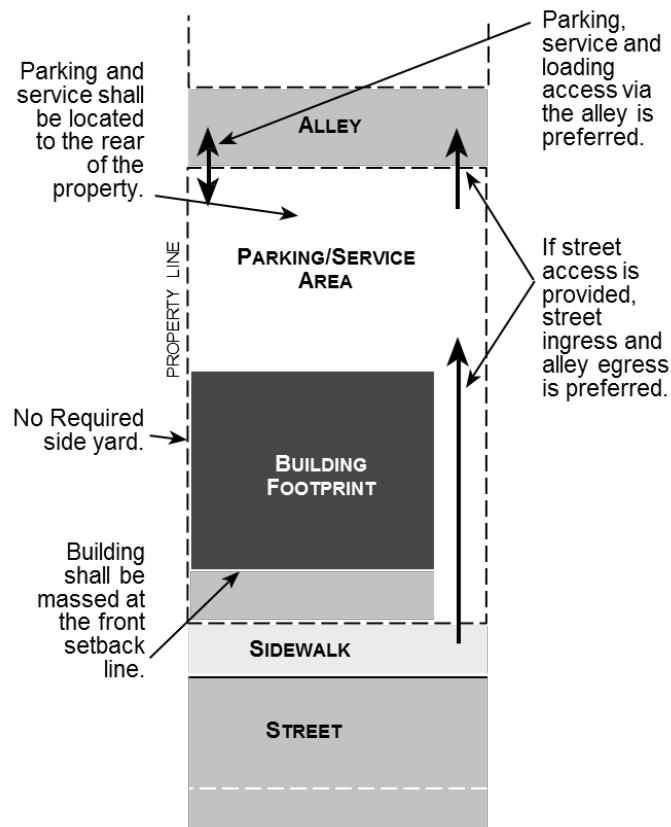
- (1) Building orientation should provide for at least one significant street entrance elevation. Other frontages should have a pedestrian scale and be softened by landscaping.

### Setbacks

- (1) This district is characterized by varying depths of landscape setbacks around its signature buildings. Setbacks shall be relatively shallow, with breaks to provide public access.

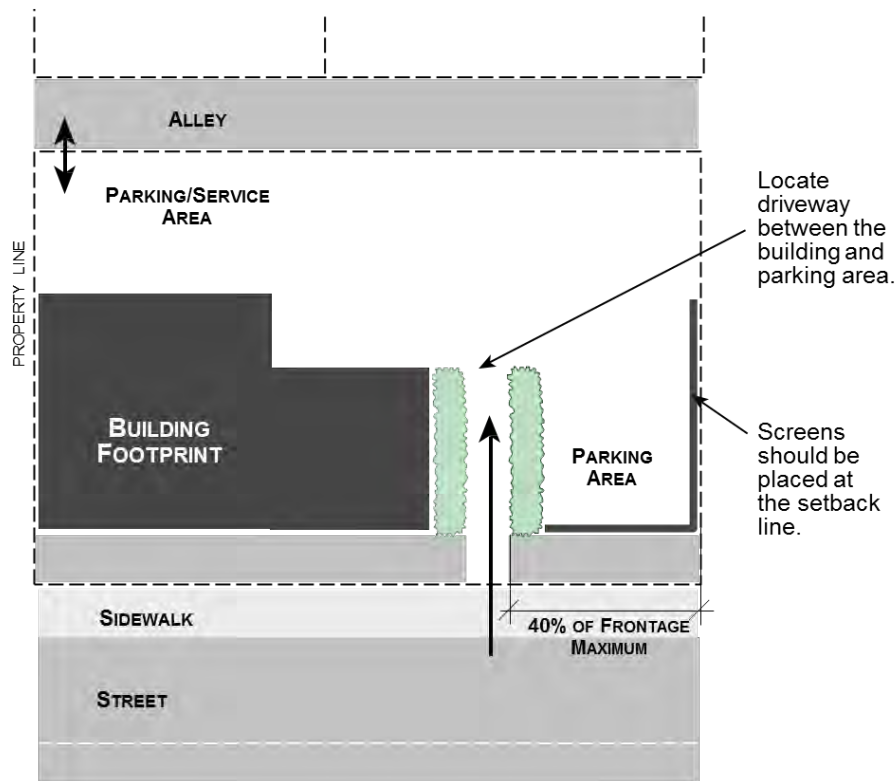
### Vehicular Access and Parking

- (1) Parking shall typically be provided in parking garages with limited numbers of common access points.
- (2) On site parking (lots and structures) shall be located to the rear of the building for parcel widths less than 200 feet and accessed by alleyways wherever they exist.



Site Layout including driveway locations for parcels less than 200 feet in width.

- (3) For parcel widths in excess of 200 feet, parking lots and structures may occupy up to 40% of the parcel's street frontage.



Site Layout including driveway locations for parcels more than 200 feet in width.

### **Pedestrian Access**

- (1) Pedestrian access may be directly from the street or via a courtyard having a significant frontage on a street.
- (2) Pedestrian access through or beneath buildings is appropriate in this district and is encouraged.

### **Site Furniture**

- (1) Pedestrian amenities are encouraged, including benches, landscaped gathering areas, trash receptacles, etc. Design of improvements should be contemporary with traditional references.

### **Courtyards and Passages**

- (1) Courtyards and pedestrian passages are encouraged to facilitate pedestrian circulation and to provide resting and gathering places. Design of improvements should be contemporary with traditional references.

### **7.6.3 Architecture**

#### **Style**

- (1) Existing buildings should be restored/maintained in a style that reflects the original appearance of the building.
- (2) This district should exhibit a modern “formalness” befitting government and judicial uses. Government buildings should exhibit a formal character through contemporary architectural forms. The architectural style of new buildings should have a contemporary appearance but utilize elements that complement the historic character of Riverside. New buildings should be contemporary using the signature buildings as a source of design inspiration.

#### **Scale**

- (1) The portion of buildings and improvements with an orientation to pedestrian spaces should be at a pedestrian scale. Other parts of buildings, which do not include pedestrian spaces, may be high-rise.

#### **Detailing**

- (1) Detailing of existing buildings should be accurate to the original style of the building. New buildings should use detailing reflective of the signature buildings in the district.

#### **Roof Design**

- (1) Roof design should reflect/complement significant buildings in the area.

#### **Colors and Materials**

- (1) Muted earth tones and traditional materials should prevail, with brighter colors limited to trim areas. The Bankruptcy Court is a good example of this approach.

### **7.6.4 Landscaping**

#### **Plant Types**

- (1) Plant types may be drawn from a broad plant palette, however, they should be types suitable for the climate and the exposure in which they are to be placed.

#### **Scale**

- (1) Pedestrian scale plantings should prevail, with larger plantings used as accents.

#### **Relationship to Development**

- (1) Plantings in courtyards should be at a pedestrian scale, accenting and complementing building architecture.
- (2) Plantings near taller building elements should be taller and/or larger in scale so as to relate to the view seen from a distance.

#### **Hardscape**

- (1) Paved areas may draw from a broad range of materials, designs, and finishes that are complementary to the building architecture.



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### **7.6.5 Signs**

#### **Style**

- (1) Contemporary designs that are low-key and complementary to the building's architecture are encouraged.

#### **Scale**

- (1) Signing should be an accent to the architecture. The overall effect should be low key and proportional to the building. Signs at the first floor should be the smallest and most low key. Signs at the top of the building should be large enough to be seen from a distance.

#### **Type**

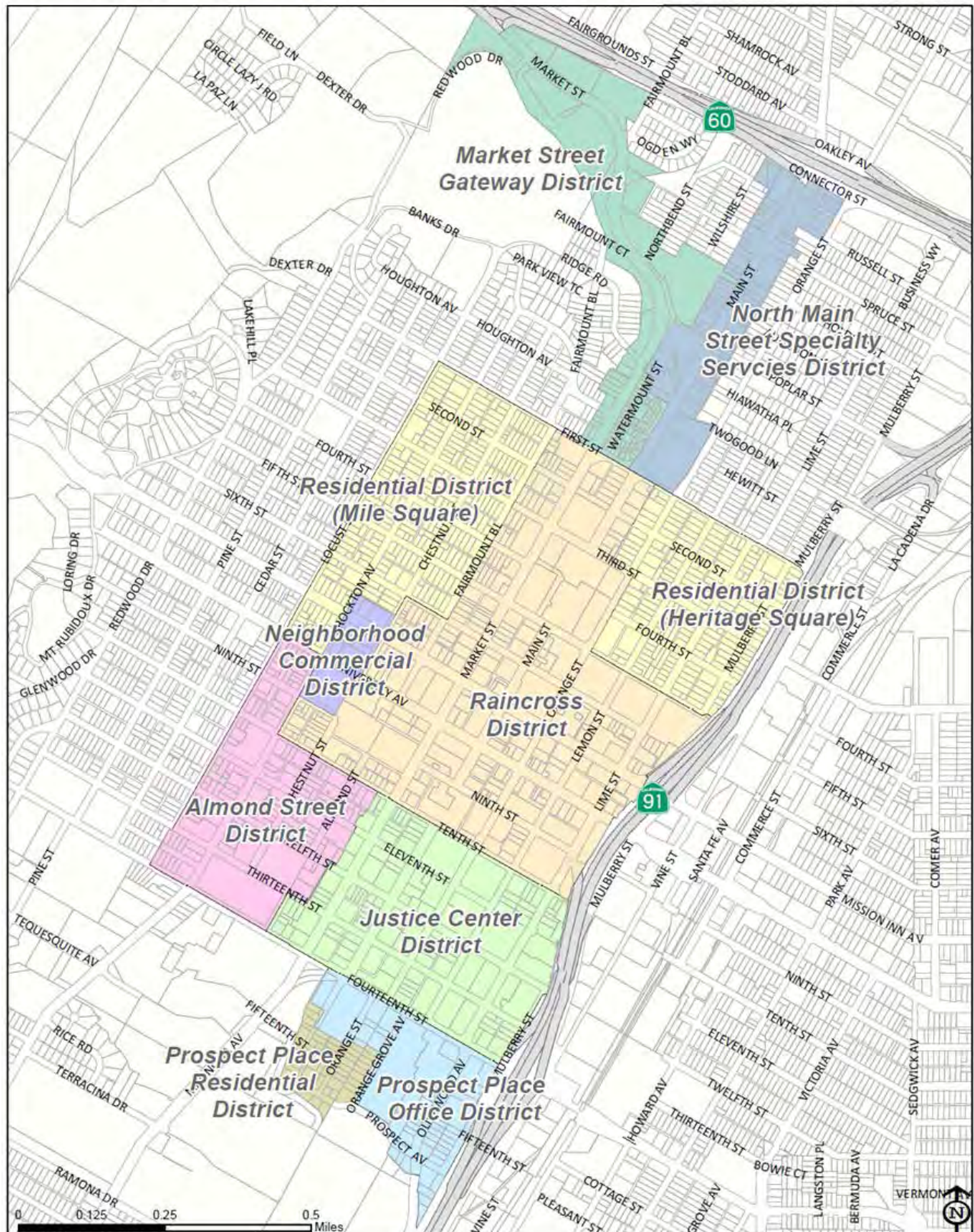
- (1) Signs should be contemporary in nature. Acceptable types include channel letters, reverse channel letters, pegged-on metal letters, routed metal with push-through plastic letters. Plastic faced canister signs, wood signs, and painted signs are generally not appropriate.

### **7.6.6 Additional Design Standards and Guidelines**

Refer to Chapter 15 for general design standards and guidelines that apply to the entire Downtown Specific Plan area.

# P16-0854 Downtown Specific Plan Amendment

## Exhibit 4: Downtown Specific Plan Districts







**Community Development Department**

3900 Main Street, Riverside, CA 92522 | Phone: (951) 826-5371 | [RiversideCA.gov](http://RiversideCA.gov)

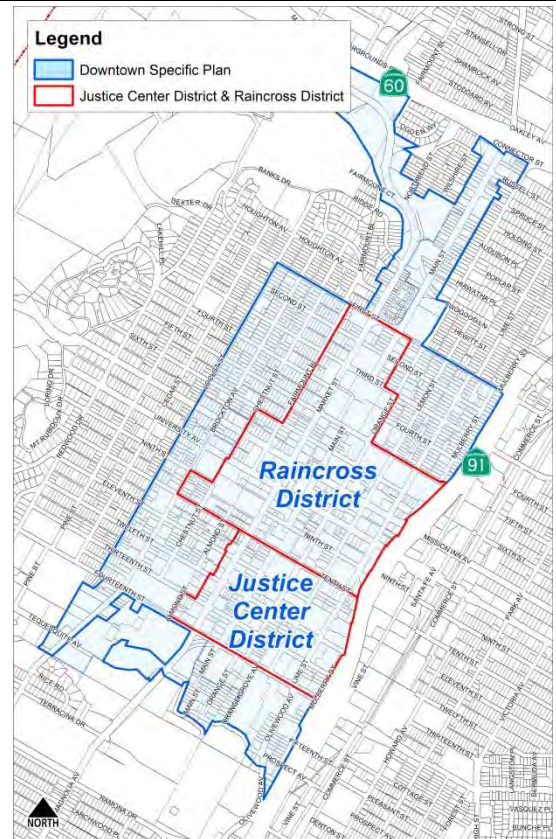
**Planning Division**

**PLANNING COMMISSION WORKSHOP DATE: MAY 7, 2015**

**AGENDA ITEM NO.: #6**

**PROPOSED PROJECT**

<b>Case Numbers</b>	N/A
<b>Request</b>	Downtown Specific Plan Workshop and Discussion Regarding Urban Design Concepts and Trends
<b>Applicant</b>	City of Riverside, Community & Economic Development Department - Planning Division
<b>Project Location</b>	Downtown Specific Plan Area, Raincross District and Justice Center District
<b>Project area</b>	Approximately 250 acres
<b>Ward</b>	1
<b>Neighborhood</b>	Downtown
<b>Specific Plan</b>	Downtown Specific Plan
<b>General Plan Designation(s)</b>	DSP-RC – Downtown Specific Plan Raincross District DSP-JC – Downtown Specific Plan Justice Center District
<b>Zoning Designation(s)</b>	DSP-RC – Downtown Specific Plan Raincross District DSP-JC – Downtown Specific Plan Justice Center District
<b>Staff Planner</b>	David Murray, Senior Planner; 951-826-5773; <a href="mailto:dmurray@riversideca.gov">dmurray@riversideca.gov</a>



## WORKSHOP PURPOSE

The purpose of this workshop is to provide the Planning Commission with an update regarding the Downtown Specific Plan, and to engage the Commission in a discussion about urban design and land development. This workshop is intended to explore topics such as land use trends, transit and urban mobility, parking demands, walkability, and pedestrian amenities.

## BACKGROUND

### Downtown Specific Plan

Adopted in 2002, the existing Downtown Specific Plan (*available for review at <http://www.riversideca.gov/planning/cityplans-csp-downtown.asp>*) was created to facilitate development and improvements that realize the community's vision for Downtown, and reinforce Downtown Riverside as the cultural, arts, retail, and entertainment center of the Inland Empire. The Specific Plan also seeks to strengthen the Downtown as a distinctive center for citizens and visitors by encouraging attractive design, enhanced public spaces, mixed-use developments, historic preservation and a variety of housing options. Based on a community-developed vision, the Specific Plan strives to meet these goals with detailed development regulations, design standards, design guidelines, and land use regulations, intended to create a tool for public improvement projects, promote development, evaluate development proposals and facilitate historic preservation.

The Downtown Specific Plan (DSP) area consists of approximately 640 acres. It is divided into eleven primary Land Use Districts, each with their own unique development patterns and character (see Exhibit 1). The Raincross District and Justice Center District are the heart of Downtown, and are historically the social, cultural and civic core of the City. These two districts are the focus of this staff report, as they will be the focus of a future (anticipated) update to the Downtown Specific Plan.

The *Raincross District* encourages a concentration of retail, office, cultural, residential, and visitor serving uses that generate activity during both daytime and evening hours. These uses may be within a single-use or mixed-use building. The center of the District is occupied by the Mission Inn Historic District, which is an "overlay" zone that contains Riverside's most important historic buildings. This overlay (sub-district) has been created to maintain a scale of development that is compatible with the established historic fabric of the area. The Mission Inn Historic District contains additional development standards that have been carefully crafted to achieve this goal.

The Raincross District is further characterized by the numerous local and national historic landmarks that define the areas character, including the Mission Inn, Fox Theater, Stalder Building, Municipal Auditorium, Municipal Museum, Unitarian Church, Congregational Church, Post Office, Loring Building, and Riverside Art Museum. Preservation of these structures, along with careful and compatible design of new development, is important in maintaining the areas character and unique identity.

The *Justice Center District* is anchored by the historic Riverside County Courthouse. This District provides a center for the existing and future expansion of major judicial uses, including Federal, Appellate, State and County courts. This District allows for a concentration of office uses and business supply retail uses to maintain Downtown as the City's employment center. Under the existing Specific Plan, the Justice Center is intended to encourage medium to high density office

uses to support and complement the retail, cultural, entertainment and residential uses of the nearby Raincross District.

### **Development Trends Downtown**

Consistent with the City's "*Seizing Our Destiny*" effort to make Riverside a city of choice, Downtown Riverside has experienced a series of improvements in the past few years. This includes the openings of the renovated Fox Performing Arts Center, the Fox Entertainment Plaza, the Municipal Auditorium, the expansion and renovation of the Riverside Convention Center, the Culver Center for the Arts, the Citrus Tower office building, and the Hyatt Place Hotel. As a result, Downtown has also seen a recent influx of exciting new dining and entertainment venues and more people to the area. This investment in the Downtown core is expected to continue, as demonstrated by the residential and mixed-use developments currently under review by Planning staff, the construction of Riverside Community College District's Culinary School (located at the corner of Market Street and University Avenue), countless informal discussions with developers and investors who see value in the urban center, and the trends of Millennials to live in vibrant urban lifestyle.

### Downtown Retail & Entertainment Strategy

In 2012 the City Council approved a Downtown Retail & Entertainment Strategy, which included an analysis of Downtown's strengths and opportunities for growth. This strategy laid out the following recommendations, which are intended to be included in a future Downtown Specific Plan update:

- a. Encourage expansion of eating, drinking and entertainment establishments; concentrate entertainment venues in a compact area
- b. Encourage the expansion of existing business and recruit local and regional retailers
- c. Increase linkages within and to the Downtown
- d. Improve linkages to public transportation and the Downtown
- e. Discourage additional social services from locating or expanding in the Downtown
- f. Integrate multi-family housing into the Downtown land use mix
- g. Streamline and make user-friendly the process for opening a business in Downtown
- h. Conduct an annual contest to attract unique and experiential concepts to the Downtown
- i. Increase events and programming to activate the street scene
- j. Improve signage and wayfinding systems, reduce perceived hassle of parking in the Downtown, and improve appearance of arrival gateways to Downtown
- k. Implement a more visible and cohesive branding program

Furthermore, the City Council has given the following direction:

- Update the Downtown Specific Plan and consider:
  - Clustering of "like" uses
  - Improve downtown linkages
  - Focus future development around centerpieces such as Main Street Riverside (pedestrian mall) and the Convention Center; and
  - Improve Downtown circulation
- Develop a plan to bring more rooftop gardens to the Downtown; and
- Monitor impacts of improvements on Downtown parking and new garages

## Downtown Specific Plan Visioning Workshops

In 2012 Planning Division Staff also organized a series of visioning sessions intended to define the types of dining, entertainment and public amenities desired by residents, business owners, and visitors of Downtown. The results of these sessions revealed a strong desire for a wide variety of businesses and amenities. Generally speaking, participants expressed interest in creating a vibrant and active Downtown that appeals to a wide variety of patrons, by maintaining a balance of family-oriented activities and nighttime entertainment activities, and by clustering like uses. Additionally, it was recognized that there are a number of sensitive receptors within and adjacent to the Downtown core, such as residential and hospitality uses. This was identified as an important consideration when establishing location and development standards for nighttime entertainment venues.

Given the recent interest in developing residential, dining and nighttime entertainment uses in the Downtown, and the City's desire to encourage such uses, the City Council and staff have identified a need to take a proactive look at the Downtown Specific Plan and develop a vision that accounts for development trends and housing needs, while remaining sensitive to the Downtown's historic fabric. Additionally, there are opportunities to revise the development standards, design guidelines, and permitted uses within the Raincross District and Justice Center District to facilitate growth while achieving the stated community vision.

### **Housing Element**

As required by state law, a Housing Element was prepared as part of General Plan 2025. The Housing Element establishes goals, objectives, policies, and programs to facilitate the development, improvement, and preservation of housing. It is intended to create livable neighborhoods that offer a high quality of life; facilitate a diversity of housing choices for different lifestyles; increase housing opportunities for very low, low and moderate income households; and support the provision of adequate housing and supportive services for those with special needs.

California law requires Housing Elements to address the housing needs of projected population and employment growth. The Southern California Association of Governments (SCAG) prepares growth forecasts for the region, and identifies the amount of housing Southern California will need to accommodate as it grows. Per the California Government Code, SCAG is the agency responsible for implementing the Regional Housing Needs Assessment (RHNA), a State mandated program that requires each City to accommodate a fair share of housing opportunity. The way the RHNA works is that SCAG forecasts the amount of housing needed to accommodate region growth, and then assigns portions of the forecasted housing to each of the local jurisdiction based on their anticipated growth forecasts. Cities (and Counties) then must verify that they have zoned enough land to accommodate the number of housing units required. Furthermore, it is important to note that the RHNA allocations included different economic levels of homeownership, including housing for families of low and very low income. If a local jurisdiction does not have enough residentially zoned properties to accommodate the forecasted growth, then they are required to develop proactive policies and programs to accommodate new housing construction that achieves the State's housing goals. At a State level, the primary objective is to ensure that there is enough land available to accommodate the housing needs of lower income families.

For the City of Riverside's 2014-2021 Housing Element, the City's total allocated housing need for lower-income households is 6,077 dwelling units. After accounting for projects that are "in the pipeline", and the underutilized sites currently zoned for residential development, the City has a

remaining RHNA obligation of 4,767 lower-income household units. This means that the City's current zoning is inadequate to meet the minimum level of State mandated housing, and therefore the City is obligated to amend its Zoning regulations and maps to allow for higher density housing. To accommodate the 4,767 units of lower-income households, the City must identify at least 191 acres of vacant or underutilized land at an average density of at least 25 units per acre that was either, (1) currently zoned for residential development and not previously identified in the 2012-2021 Housing Element, or (2) can be rezoned to a residential zone that achieves this density.

An important distinction, and often a misconception, is that the RHNA obligation for 4,767 units is a requirement to "accommodate" lower-income housing. The City is obligated to zone for, encourage, and remove barriers that prevent the construction of these units; the City is not obligated to have 4,767 units physically constructed.

Because of the densities associated with the RHNA allocation, and the number of units that need to be accommodated, the Downtown and the adjacent Marketplace Specific Plan area will be crucial in meeting the City's obligation, since these areas can accommodate the greatest densities in the City, and will likely have the least impacts to established lower density neighborhoods. Assuming an average density of 40 units per acre within these areas, staff seeks to identify roughly 60 acres of vacant or underutilized property within the Downtown and Marketplace areas that will accommodate as many as 2,400 additional units. While the City can encourage additional residential units in the downtown areas, it is important to understand the RHNA criteria only allows 50% of the required units to be within mixed-use zones. The other 50%+ units must be located on property that is zoned exclusively for multi-family residential use.

It should be noted that adding 2,400 dwelling units in Downtown is not a new prospect, as the City Council has previously established policy that the Downtown is to accommodate "thousands" of additional housing units. To achieve the Council directive and comply with State Housing Element law, city staff has initiated an effort to identify vacant and underutilized commercial, office and industrial properties in the Downtown that could be adaptively reused or demolished. While the City's current zoning allows residential units in downtown, either as multi-family housing or mixed-use developments, it is clear that the existing Zoning Code or Downtown Specific Plan needs to be amended to eliminate barriers that hinder residential investments.

### **Development Standards**

New residential, retail, office and mixed-use development prescribed by the Specific Plan can bring increased populations into the area. This additional population drives a demand for support services, which spurs further economic investments. Furthermore, new development that is done well, and furthers a sense of place, can draw new visitors, occupants and investors to an area that otherwise would not have seen the Downtown as a desirable location.

Although the current Downtown Specific Plan lays a foundation for a 10 to 20 year growth in Downtown, there are opportunities to refine Specific Plan development standards to simplify and incentivize economic development. Specifically, staff recognizes the need to evaluate the permitted uses, conditionally permitted uses, residential densities, building heights, and parking requirements, as well as public improvements such as pedestrian enhancements, bicycle amenities, and inviting public gathering spaces.

## Land Uses

A vibrant Downtown provides for a multitude of uses that cater to diverse users. Residential, retail, and office uses are vital components of a dynamic downtown, but must be logically integrated to create a healthy and successful environment. With this in mind, staff continues to look at ways to retain cultural facilities and established businesses, while encouraging restaurant, entertainment, and residential investment that generate a higher saturation of evening and weekend activities, increase revenues and act as a catalyst for future investment.

While the Downtown Specific Plan currently allows for a variety of uses, and the City has seen Downtown investment in recent years, the current levels of investment are far below the Downtown's potential.

To reach its potential, staff believes there is a need to streamline the approval and permitting process to accommodate desirable activities in key locations, and to consider revisions to the uses allowed within the Raincross and Justice Center Districts. Over the coming months staff will begin to explore an update to the DSP to consider issues such as...

- Identify the potential boundary for an Entertainment District. This area would potentially encourage dining and entertainment uses through reduced permit requirements or streamlined approval process;
- Identify and remove existing barriers to dining and entertainment uses;
- Evaluate the permitting requirements for entertainment uses, particularly within the Justice Center District;
- Identify necessary restrictions to entertainment businesses to ensure they remain compatible with residential and office uses, and don't generate a negative perception of Downtown or burdens on downtown investment;
- Explore opportunities for residential projects within the Justice Center District;
- Identify appropriate locations to encourage residential uses as part of mixed-use developments, in order to meet the needs of the Housing Element;
- Explore and identify adaptive re-use opportunities of historic and underutilized properties;
- Ensure visitor safety through urban design and programmatic community watch programs;
- Identify opportunities for place-making in public and quasi-public spaces through land use development; and
- Investigate opportunities to complement and enhance public parking programs, including the "right-sizing" of private parking requirements, shared parking concepts and parking management principals.

## Densities and Scale

In anticipation of the continuing growth of Downtown and the desired mix of uses, staff must carefully consider appropriate densities along with the massing and scale of new buildings, while remaining sensitive to the historic fabric. With continued mandates from the State, such as the Regional Housing Needs Assessment (RHNA) allocations, the City is consistently under pressure to identify sites for high density residential uses that could accommodate low- to moderate-income housing needs. It is apparent that the most logical place for dense residential is within Downtown, and as such, staff is actively exploring opportunities for rezoning and intensifying residential densities to satisfy the mandated requirements.

Currently the maximum density for residential units within the Raincross District is 60 units per acre (residential is not permitted within the Justice Center District). Although there are also density



bonuses available, there may be other opportunities to increase this density limit to accommodate more units.

For office and commercial uses, density is measured using Floor Area Ratio (FAR), which is calculated by dividing all the building square footage on a site by the property's square footage. Within the Raincross District, the maximum FAR is anywhere from 3.0 to 4.5, depending on location; and within the Justice Center District the maximum FAR is between 4.0 and 6.0. Staff will evaluate the current standards and look for ways to encourage growth to these limits, and possibly increase the maximum FARs within these districts.

In conjunction with increased densities and FAR, staff will need to explore the appropriate scale or height for the Downtown to adequately accommodate increases in existing limits. Currently, depending on the location, and possibly with the granting of a Conditional Use Permit, the maximum height within the Raincross District is between 50 and 100 feet, and between 60 and 140 feet within the Justice Center District. Additionally, the Justice Center District includes a *minimum* height of 45 feet for at least 50% of the building footprint for properties outside the Mission Inn Historic District. This standard is based on a strategy to more specifically guide development and density, and achieve the desired "form" where it is most appropriate. Where appropriate, staff may explore additional opportunities to establish "minimum" heights or densities throughout the Downtown Specific Plan update.

### Parking

One of the most significant challenges to economic development within Downtown is parking. Although parking requirements are exempted for uses in an existing structure designated a historic resource, or contributor to an historic district, new development and expansions to a historic structure must provide on-site parking per the ratios outlined within the Downtown Specific Plan. In some cases, as is the case with residential uses, the parking requirement in the Downtown Specific Plan defers back to the City's Zoning Code. This means that the City requires parking in Downtown at the same rate as that required in the suburban areas, without consideration of the unique factors of downtown urban living.

As the City moves toward a more mixed-use urban core, and strives to reduce the reliance on the automobile, air pollution, and greenhouse gas emissions, accommodations for reduced parking standards need to be considered in those areas that have a walkable environment and are served by public transit, such as the Downtown Specific Plan. What is normal in a traditional multiple-family residential project in the suburbs of the City is not the same fit for what is needed in Downtown.

Unlike most areas of the City, Downtown Riverside is a composition of uses that are within easy walking distance of each other. Downtown is also well served by RTA's bus service, which has connections to shopping, dining, education and employment centers throughout the City. Additionally, the City is currently studying the feasibility of a streetcar system that could provide linkages to other areas of the City; and the Downtown Metrolink Station is within walking proximity to the Downtown core, providing direct connections to the La Sierra and Hunter Park areas of the City, as well as Los Angeles, Orange and San Bernardino Counties.

Taking these facts into consideration, along with an emerging trend for Millennials to embrace urban living and alternative modes of transportation, staff intends to explore opportunities to "right-size" the parking requirements during the Downtown Specific Plan update. This is consistent with the Downtown Specific Plan itself, which recognizes the need to periodically review the parking requirements with an emphasis on the "park once" concept.

Additionally, if it is the City's intention to facilitate and encourage private investment in downtown, the City will need to adopt parking regulations that meet the needs of the public while addressing the realities of the real estate market. Because of the nature of downtowns, which achieve higher densities through compact development, excessive parking requirements have significant implication on whether a project is economically viable. As an example, the cost of one parking space generally ranges from \$10,000 to \$40,000, depending on whether it is a surface lot or in a subterranean garage (and costs can range significantly depending on soil conditions, figurations in material and labor costs, etc.). If the City requires a developer to build one parking space per unit than is needed to achieve the public's parking demand, and the project has a density of 40 unit per acre, then the developer could easily be spending millions of dollars on infrastructure that is not needed and may not be used.

As the City moves forward with "right-sizing" parking in Downtown, staff intends to evaluate the actual parking demands for downtown developments (resident needs, construction loan requirements, sell-ability, etc.), and identify methodologies to improve parking efficiencies and provide personal transit options . Some of these potential methodologies include shared parking arrangements (i.e., office use during day and residential parking at night), parking management programs, unbundling, automated facilities, and car and NEV sharing.

### **Summary**

As the City continues to see increasing interest for development Downtown, and attempts to facilitate and encourage development to achieve state and regional requirements, the Planning Division must revisit the standards of the Downtown Specific Plan. To accommodate growth the City must look toward densification and intensification of uses and structures. Additionally, focusing development and growth within the core reduces the demands and impacts on other areas of the City, reduces the need to expand infrastructure and utilities, and results in a more sustainable environment. Providing an environment where there are options to personal automobiles encourages more compact and efficient development, encourages a walk-able and bike-able place, and ultimately reduces the negative products associated with vehicle miles traveled.

Perhaps most importantly, having a focused vision and clear, logical development standards will result in increased economic development within Downtown, which will in turn attract more investment and provide for a quality environment.

## **EXHIBITS LIST**

### **Exhibit 1 – Downtown Specific Plan Districts**

Report Prepared by:	David Murray, Senior Planner
Report Reviewed by:	Jay Eastman, Principal Planner
Report Approved by:	Emilio Ramirez, Interim Community Development Director