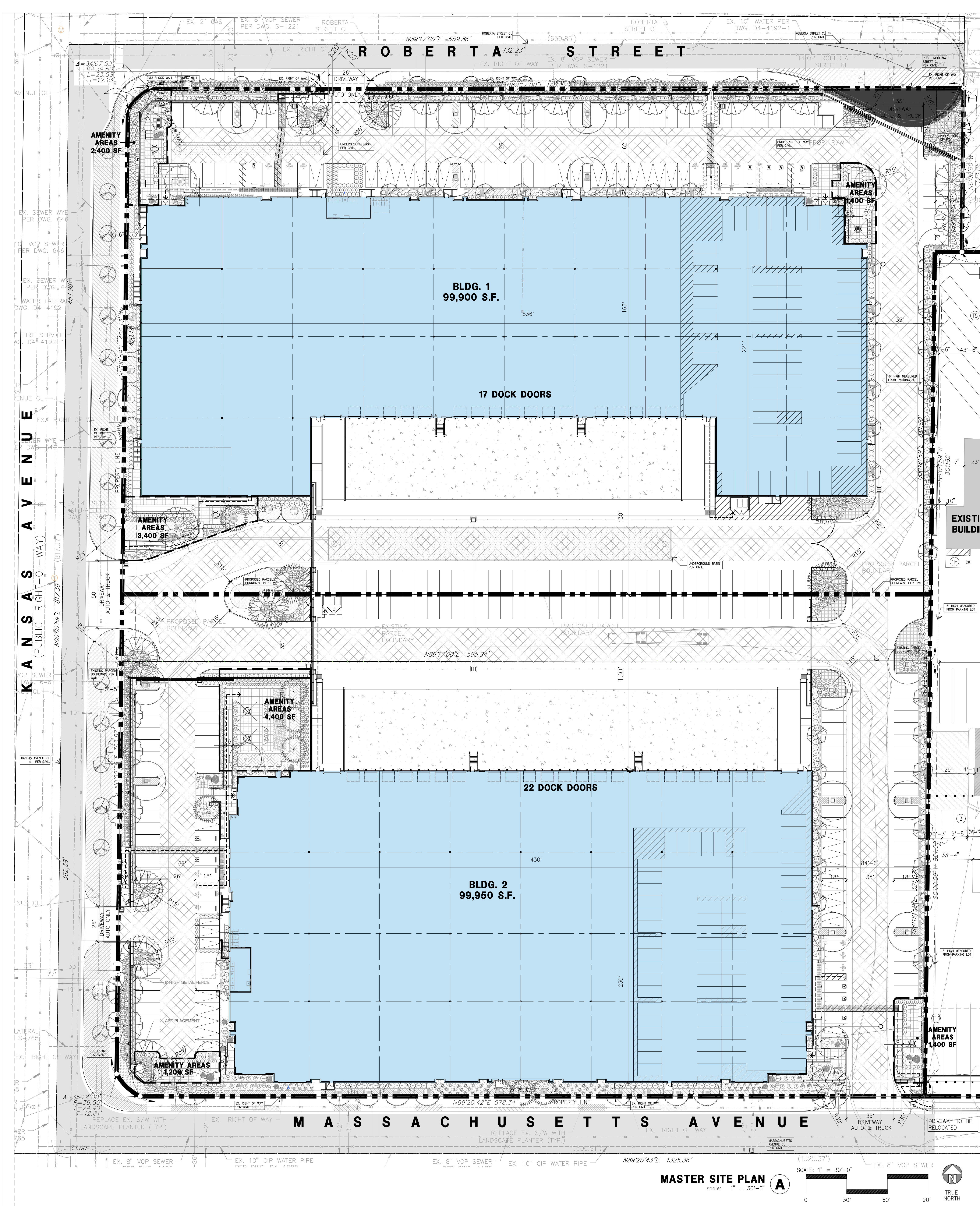




PROPERTY OWNER

STALEY POINT CAPITAL
MIKE KANG
11150 SANTA MONICA BLVD, SUITE 700
LOS ANGELES, CA 90025
CONTACT: T: 310-929-8097
M: 310-528-2230
EMAIL: MIKE@STALEYPOINT.COM

ADDRESS OF THE PROPERTY

2626 KANSAS AVENUE, 1989 AND 2069 MASSACHUSETTS AVENUE,
RIVERSIDE, CA 92507

ASSESSOR'S PARCEL NUMBER

210-130-015-1, 210-130-016-2

ZONING

INDUSTRIAL (I)

APPLICANT

STALEY POINT CAPITAL
MIKE KANG
11150 SANTA MONICA BLVD,
SUITE 700
LOS ANGELES, CA 90025
CONTACT: T: 310-929-8097
M: 310-528-2230
EMAIL: MIKE@STALEYPOINT.COM

APPLICANT'S RESPRESENTATIVE

HPA INC.
18831 BARDEEN AVE., SUITE 100
IRVINE, CA 92612
CONTACT: ANDY WIYANTO
TEL: 949-862-2161

AERIAL MAP



AMENITY AREAS CALC.

BUILDING 1
AREA A = 2,400 SF
AREA B = 1,400 SF
AREA C = 3,400 SF
TOTAL = 7,200 SF

BUILDING 2
AREA A = 4,400 SF
AREA B = 1,200 SF
AREA C = 1,400 SF
TOTAL = 7,000 SF

PROJECT DATA

SITE AREA	BLOS. 1		BLOS. 2		TOTAL
	BLDG. 1	BLDG. 2	BLDG. 1	BLDG. 2	
BLDG. 1 AREA	224,180	220,302	444,772	s.f.	
BLDG. 2 AREA	5,15	5,08	10,21	sq.	
Existing	0	0	0		
Office - 1st floor	6,000	6,000	12,000	s.f.	
Office - 2nd floor	0	2,250	2,250	s.f.	
Warehousing	28,170	27,510	55,680	s.f.	
Manufacturing	68,700	64,180	132,880	s.f.	
TOTAL	99,900	96,950	196,850	s.f.	
FAR	0.445	0.454	0.899		
AUTO PARKING REQUIRED	24	33	57	spaces	
Office - 1000 s.f.	28	28	56	spaces	
Warehouse - 1500 s.f.	131	128	259	spaces	
TOTAL	155	159	312	spaces	
AUTO PARKING PROVIDED	70	70	140	spaces	
Standard (8' x 18')	100	100	200	spaces	
Standard Accessible Parking (8' x 18')	3	4	7	spaces	
Van Accessible Parking (12' x 15')	2	1	3	spaces	
EVCS (8' x 18')	19	19	38	spaces	
EVCS (8' x 18')	4	4	8	spaces	
EVSE Standard Accessible (8' x 18')	1	1	2	spaces	
EVSE Van Accessible (12' x 18')	1	1	2	spaces	
EVCS	200	200	400	spaces	
EVSE	3	3	6	spaces	
LANDSCAPE REQUIREMENT					
Percentage - City					
ZONING ORDINANCE FOR CITY					
Forestry Designation - Industrial					
MAXIMUM BUILDING HEIGHT ALLOWED					
Height - 45'					
MAXIMUM FLOOR AREA RATIO					
FAR - 0.5					
SETBACKS					
Building					
Front - 20'					
Interior Side - 0'					
Rear Side - same as front					
Rear - 0'					

LEGAL DESCRIPTION

PARCEL 1: (APN : 210-130-015-1)
ALL THAT CERTAIN PORTION OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 24, TOWNSHIP 2 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF RIVERSIDE, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, DESCRIBED AS FOLLOWS:
COMMENCING AT THE SOUTHWEST CORNER OF SAID NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SAID SECTION 24; THENCE ALONG THE WESTERLY LINE OF SAID NORTHWEST 1/4 OF THE NORTHEAST 1/4 NORTH 0°00'59" EAST 817.37 FEET TO THE CENTERLINE OF ROBERTA STREET AS SHOWN BY A MAP OF RECORD OF SURVEY ON FILE IN BOOK 73, PAGE 17 OF RECORDS OF SURVEY, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA; THENCE ALONG SAID CENTERLINE OF ROBERTA STREET NORTH 89°17'00" EAST 659.86 FEET; THENCE SOUTH 0°00'50" EAST 20.00 FEET MORE OR LESS, TO THE NORTHEAST CORNER OF LOT II OF THE MARIE TRACT AS SHOWN BY THE ABOVE - MENTIONED RECORD OF SURVEY, SAID POINT BEING THE TRUE POINT OF BEGINNING; THENCE ALONG THE EASTERLY LINE OF SAID LOT II, SOUTH 0°00'50" EAST 133.65 FEET TO THE SOUTHEAST CORNER THEREOF, SAID SOUTHEAST CORNER BEING THE SOUTHWEST CORNER OF THE NORTHEAST 1/4 OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SAID SECTION 24, AS SHOWN BY ABOVE - MENTIONED RECORD OF SURVEY; THENCE SOUTH 89°17'00" WEST 20.00 FEET; THENCE SOUTH 0°00'59" WEST 301.32 FEET; THENCE SOUTH 89°17'00" WEST 606.92 FEET, MORE OR LESS TO A POINT 33 FEET EASTERLY OF THE WESTERLY LINE OF SAID NORTHWESTERLY 1/4 OF THE NORTHWESTERLY 1/4; THENCE NORTH 0°00'59" EAST 434.97 FEET TO THE NORTHWESTERLY CORNER OF LOT I OF SAID MARIE TRACT; THENCE NORTH 89°17'00" EAST ALONG THE NORTHERLY LINE OF LOTS 1 AND 4 THROUGH 11 OF SAID MARIE TRACT, 626.85 FEET, MORE OR LESS, TO THE POINT OF BEGINNING. EXCEPTING THEREFROM THAT PORTION GRANTED TO THE CITY OF RIVERSIDE, A MUNICIPAL CORPORATION BY DEED RECORDED AUGUST 12, 1983 AS INSTRUMENT NO. 1983-164247 OF OFFICIAL RECORDS. SAID LEGAL DESCRIPTION IS PURSUANT TO UNRECORDED PARCEL MAP WAIVER 16-834 AS DISCLOSED BY DOCUMENT RECORDED SEPTEMBER 16, 1986 AS INSTRUMENT NO. 1986-225835 OF OFFICIAL RECORDS.

PARCEL 2: (APN: 210-130-016-2)
ALL THAT CERTAIN PORTION OF THE NORTHWEST QUARTER OF THE NORTHEAST QUARTER OF SECTION 24, TOWNSHIP 2 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF RIVERSIDE, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:
BEGINNING AT THE SOUTHWEST CORNER OF SAID NORTHWEST QUARTER OF THE NORTHEAST QUARTER OF SAID SECTION 24; THENCE ALONG THE SOUTHERLY LINE OF SAID NORTHWEST QUARTER OF THE NORTHEAST QUARTER NORTH 89°20'43" EAST, 33.00 FEET, MORE OR LESS, TO A POINT ON THE EASTERLY LINE OF KANSAS AVENUE, SAID POINT BEING THE TRUE POINT OF BEGINNING;
THENCE CONTINUING ALONG SAID SOUTHERLY LINE NORTH 89°20'43" EAST, 606.91 FEET; THENCE NORTH 0°00'59" EAST, 363.09 FEET; THENCE SOUTH 89°17'00" WEST, 606.92 FEET, MORE OR LESS TO A POINT ON THE ABOVE MENTIONED EASTERLY LINE OF KANSAS AVENUE; THENCE ALONG SAID EASTERLY LINE SOUTH 0°00'59" WEST, 362.43 FEET TO THE POINT OF BEGINNING. EXCEPTING THEREFROM THAT PORTION LYING WITHIN THE FOLLOWING DESCRIBED PROPERTY:
THAT PORTION OF THE SOUTHERLY 1/2 OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 24, TOWNSHIP 2 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, DESCRIBED AS FOLLOWS:
COMMENCING AT A POINT IN THE WEST LINE OF SAID SOUTHERLY 1/2, LYING DISTANT THEREON SOUTH 376.32 FEET FROM THE NORTHWEST CORNER THEREOF, SAID POINT ALSO BEING THE INTERSECTION OF THE WEST PROLONGATION OF THE MOST SOUTHERLY LINE OF PARCEL 1 OF THAT CERTAIN PARCEL 1 OF LAND CONVEYED TO THE CITY OF RIVERSIDE BY DEED RECORDED AUGUST 12, 1983 AS INSTRUMENT NO. 1983-164247 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA, WITH SAID WEST LINE OF SAID NORTHEAST 1/4; THENCE NORTH 89°17'00" EAST ALONG SAID PROLONGATION 33.00 FEET TO THE MOST SOUTHWESTERLY CORNER OF SAID PARCEL 1, SAID CORNER ALSO BEING THE TRUE POINT OF BEGINNING; THENCE SOUTH ALONG THE SOUTH PROLONGATION OF THE WEST LINE OF SAID PARCEL 1, SAID SOUTH PROLONGATION ALSO BEING THE MOST WEST LINE OF PARCEL 2 OF THAT CERTAIN PARCEL OF LAND CONVEYED TO H.K. MARVIN, ET AL, BY DEED RECORDED JULY 3, 1979 AS INSTRUMENT NO. 1979-139180 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA, A DISTANCE OF 284.00 FEET TO THE MOST SOUTHWESTERLY CORNER OF LAST SAID PARCEL 2; THENCE NORTH 89°17'00" EAST ALONG THE SOUTH LINE OF SAID PARCEL 2, A DISTANCE OF 825.00 FEET TO THE MOST SOUTHEASTERLY CORNER THEREOF; THENCE NORTH A DISTANCE OF 42.00 FEET TO A LINE WHICH IS PARALLEL WITH AND DISTANT 42.00 FEET NORTH AS MEASURED AT RIGHT ANGLE FROM THE SOUTH LINE OF LAST SAID PARCEL 2; THENCE SOUTH 89°17'00" WEST ALONG SAID PARALLEL LINE A DISTANCE OF 796.37 FEET TO THE BEGINNING OF A NON - TANGENT CURVE CONCAVE TO THE NORTHEAST, HAVING A RADIUS OF 39.50 THE INITIAL RADIAL LINE AT SAID BEGINNING BEARS NORTH 25°17'32" EAST; THENCE NORTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 35°34'00" AN ARC LENGTH OF 24.52 FEET TO A LINE WHICH IS PARALLEL WITH AND DISTANT 11.00 FEET EAST AS MEASURED AT RIGHT ANGLE FROM THE MOST WEST LINE OF LAST SAID PARCEL 2; THENCE NORTH ALONG SAID PARALLEL LINE A DISTANCE OF 225.30 FEET TO THE MOST SOUTH LINE OF AFORESAID PARCEL 1; THENCE SOUTH 89°17'00" WEST ALONG SAID MOST SOUTH LINE A DISTANCE OF 11.00 FEET TO THE TRUE POINT OF BEGINNING.

NOTES:
1. DECORATIVE HARDSCAPE FINISH IS PROPOSED WHEN INTERNAL WALKWAYS CROSS DRIVEWAYS, PARKING AREAS, OR LOADING AREAS.
2. GATES TO REMAINED OPEN DURING BUSINESS HOURS



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Owner:



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LOS ANGELES, CA 90025
TEL: 310-929-8088

Project:

MASSACHUSETTS AVE.
& KANSAS AVE. BY
STALEY POINT

CITY OF RIVERSIDE, CA

Consultants:

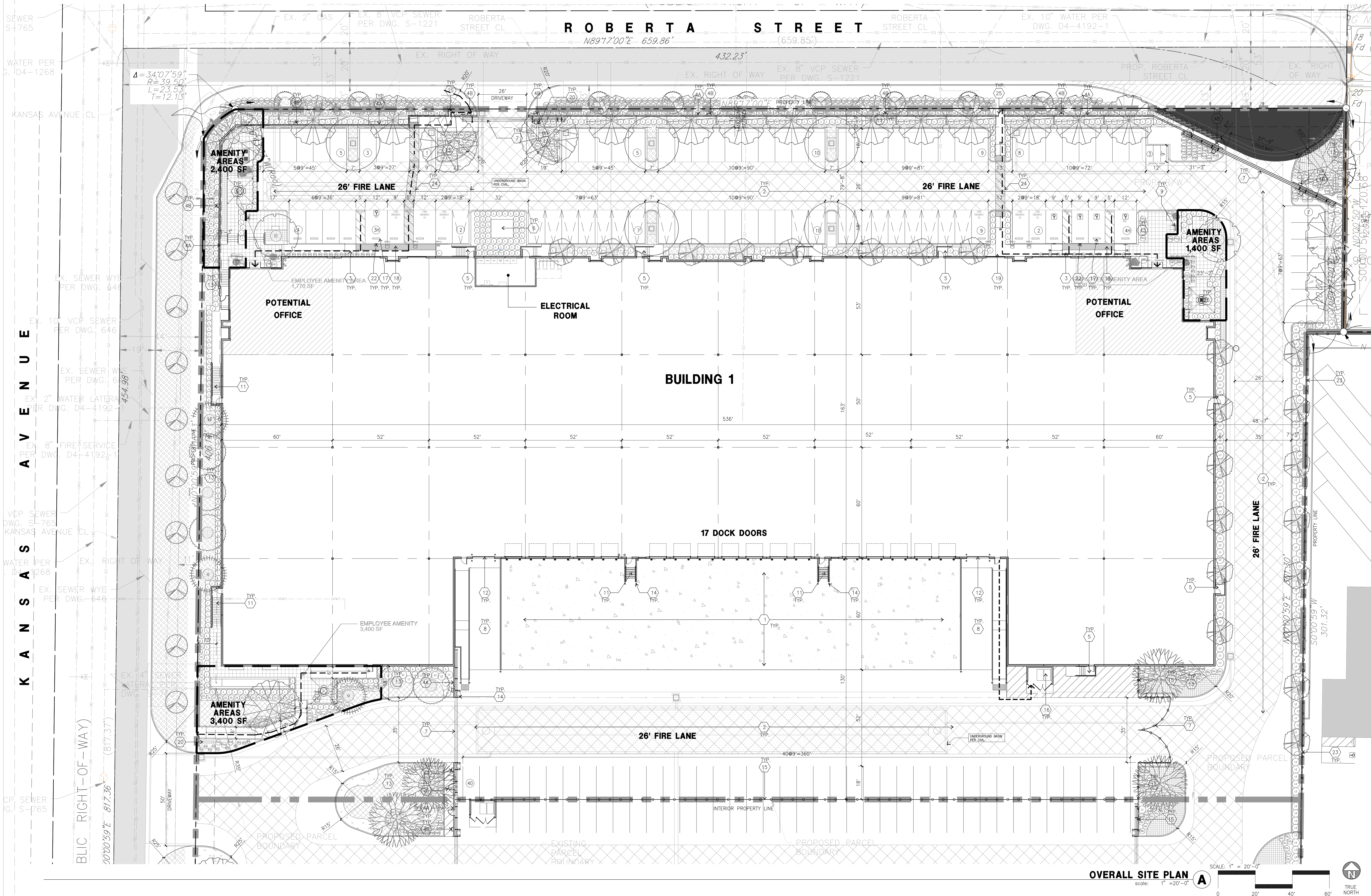
CIVIL
STRUCTURAL
MECHANICAL
PLUMBING
ELECTRICAL
LANDSCAPE
FIRE PROTECTION
SOILS ENGINEER
HUNTER

Title: MASTER SITE PLAN
(ALTERNATIVE PARKING)

Project Number: 21074
Drawn by: AW
Date: 04/11/25
Revision:

Sheet:

0-DAB-A1.0B



SITE PLAN KEYNOTES

- HEAVY BROOM FINISH CONCRETE PAVEMENT.
- ASPHALT CONCRETE (AC) PAVING.
- CONCRETE WALKWAY, MEDIUM BROOM FINISH.
A) 4" CMU BLOCK WALL WITH CAP +
PAINTED BLACK 2" TUBULAR STEEL FENCE ON THE TOP
B) 6" CMU BLOCK WALL WITH CAP (PLASTER)
-REFER TO SHEET DAB-A1.1 FOR ELEVATIONS
- 5'-6" X 5'-6" THICK CONCRETE EXTERIOR LANDING
PAD TYP. AT ALL EXTERIOR MAN DOORS TO
LANDSCAPED AREAS. FINISH TO BE MEDIUM BROOM FINISH.
PROVIDE WALK TO PUBLIC WAY OR DRIVE WAY AS REQ. BY
CITY INSPECTOR.
- TRANSFORMERS PER ELECTRICAL DRAWINGS AND SERVICE
PROVIDER.
- 6'-0" HIGH METAL SLIDING GATES W/ KNOX-BOX PER
FIRE DEPARTMENT STANDARDS PER DRIVEWAY. CONTRACTOR
TO DESIGN & DETAIL GATES, DRAWINGS AND CALCULATIONS
PRIOR TO FABRICATION. PROVIDE CONDUIT FOR FUTURE.
REMAIN OPEN DURING OFFICE HOURS.
- CONCRETE RAMP.
- 5-SPACE BIKE RACK.
- FIRE HYDRANT W/ CONC. FILLED STEEL GUARD POST.
- EXTERIOR CONCRETE STAIR.
- 12" W X 14" OVERHEAD DOOR @ DRIVE THRU.
- ALL LANDSCAPE AREAS INDICATED BY SHADING.
- CONC. FILLED GUARD POST 6" DIA. U.N.O. 42" H.
- 6" H TUBULAR STEEL FENCE.
- TRASH ENCLOSURE. REFER TO AD.7
- PRE-CAST CONC. WHEEL STOP.
- TRUNCATED DOME.
- SMOKERS' TRASH POLE.
- FUTURE MONUMENT SIGN.
- ACCESSIBLE ENTRY SIGN.
- ACCESSIBLE PARKING STALL SIGN.
- 6" H TUBULAR STEEL FENCE AT PROPERTY PERIMETER,
OFFSET 6" FROM THE PROPERTY LINE.
- COLOR CONCRETE REFER TO LANDSCAPE DRAWINGS.
- 6" H TUBE STEEL MAN GATE.
- COVERED BREAK AREA.

SITE PLAN GENERAL NOTES

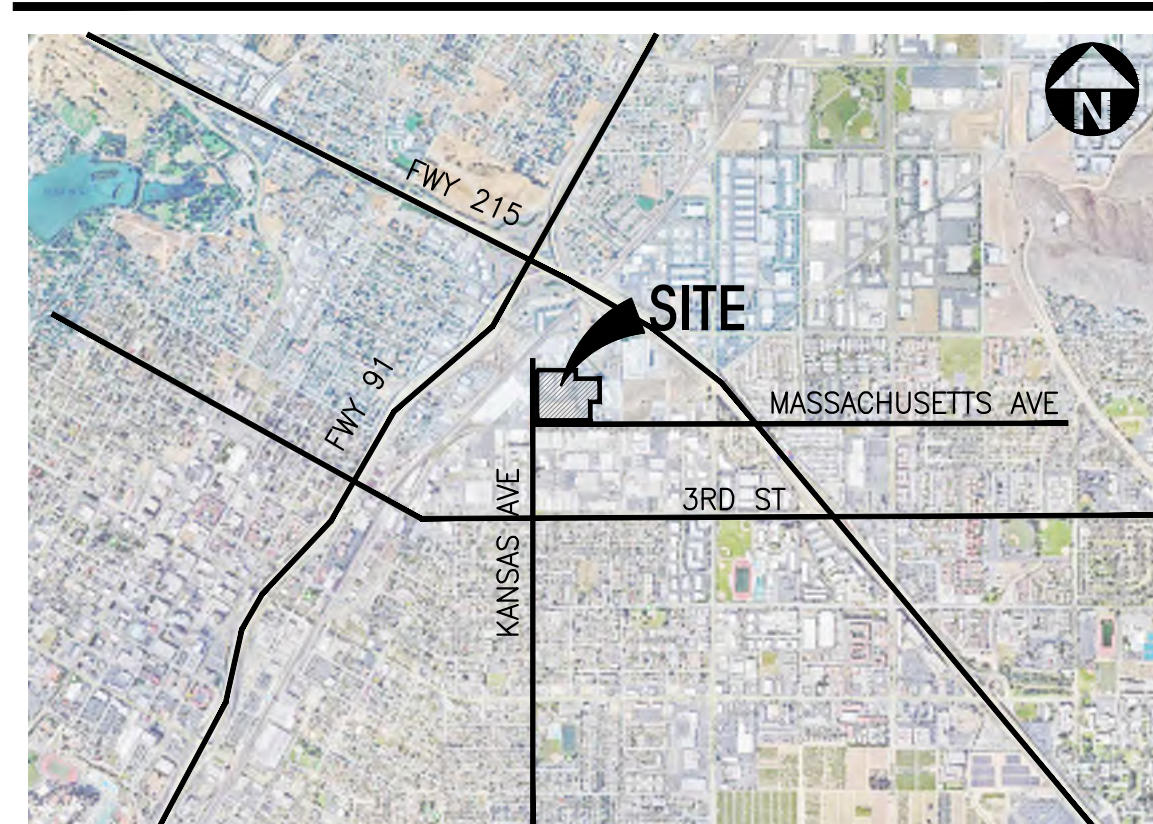
- ALL LIGHTING SHALL CONFORM WITH MUNICIPAL STANDARDS.
- SEE CIVIL AND STRUCTURAL FOR SITE CONCRETE.
- ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL, FACE OF
CONCRETE CURB OR GRID LINE U.N.O.
- REFER TO CIVIL PLANS FOR ALL CONCRETE CURBS, GUTTERS AND SWALES.
DETAILS ON SHEET AD.1 ARE MINIMUM STANDARDS.
- THE ENTIRE PROJECT SHALL BE PERMANENTLY MAINTAINED
WITH AN AUTOMATIC IRRIGATION SYSTEM.
- REFER TO CIVIL DWGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES.
CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
- PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. REFER TO CIVIL DRAWINGS.
- CONTRACTOR TO REFER TO CIVIL DRAWINGS FOR ALL HORIZONTAL CONTROL
DIMENSIONS. SITE PLANS ARE FOR GUIDANCE AND STARTING LAYOUT POINTS.
- REFER TO CIVIL DRAWINGS FOR FINISH GRADE ELEVATIONS.
- CONCRETE SIDEWALKS TO BE A MINIMUM OF 4" THICK W/ TOOLED JOINTS AT 6'
O.C. EXPANSION/CONSTRUCTION JOINTS SHALL BE A MAXIMUM 12' EA. WAY.
EXPANSION JOINTS TO HAVE COMPRESSIVE EXPANSION FILLER MATERIAL OF 1/4".
FINISH TO BE A MEDIUM BROOM FINISH U.N.O.
- ALL SIGNAGE SHALL CONFORM WITH THE MUNICIPAL STANDARD.
- PAINT CURBS AND PROVIDE SIGNS TO INFORM OF FIRE LANES AS REQUIRED
BY FIRE DEPARTMENT.
- CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND IRRIGATION
OF THE ENTIRE PROJECT SITE SHALL BE SUBMITTED TO THE BUILDING
DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO
ISSUANCE OF BUILDING PERMITS.
- PRIOR TO FINAL CITY INSPECTION, THE LANDSCAPE ARCHITECT SHALL
SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
- SITE PLAN SHALL MEET ALL ENGINEERING AND NPDES REQUIREMENT.
- ALL LANDSCAPE AND IRRIGATION DESIGNS SHALL MEET CURRENT CITY
STANDARDS AS LISTED IN GUIDELINES OR AS OBTAINED FROM PUBLIC
FACILITIES DEVELOPMENT.
- WALLS SHALL BE TREATED WITH A GRAFTITI-PROOF COATING ON SURFACES
THAT ARE NOT INTENDED TO BE PAINTED (E.G. SPLIT-FACE BLOCK WALL,
DECORATIVE TILE, COLORED PANELING MATERIAL, ETC.).
- ALL VERTICAL MOUNTING POLES OF CHAIN LINK FENCING SHALL BE CAPPED.
- LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM SIX INCHES (6")
HIGH CURB

SITE PLAN GENERAL NOTES

- | | |
|---|---|
| CONCRETE PAVING - RE: CIVIL
DRAWINGS THICKNESS | LIGHTING FIXTURE |
| STANDARD PARKING STALL
9'X18' | 26' WIDE FIRE LANE.
PROVIDE RED CURBS
AND SIGNAGE |
| ACCESSIBLE PARKING STALL
9'X18 + 5' W ACCESSIBLE AISLE | LANDSCAPE |
| VAN ACCESSIBLE
12'X18' + 5' W ACCESSIBLE AISLE | PROPERTY LINE |
| FUTURE EV CHARGING STATION
9' X 18' | ACCESSIBILITY
PATH OF TRAVEL |
| EV CHARGING STATION NEEDED
9' X 18' | |

NOTES:
1. DECORATIVE HARDSCAPE FINISH IS PROPOSED WHEN
INTERNAL WALKWAYS CROSS DRIVEWAYS, PARKING AREAS,
OR LOADING AREAS.
2. GATES TO REMAINED OPEN DURING BUSINESS HOURS

AERIAL MAP



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Owner:



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LOS ANGELES, CA 90025
TEL: 310-929-8088

Project:

MASSACHUSETTS AVE.
& KANSAS AVE. BY
STALEY POINT

CITY OF RIVERSIDE, CA

Consultants:

CIVIL
STRUCTURAL
MECHANICAL
PLUMBING
ELECTRICAL
LANDSCAPE
FIRE PROTECTION
SOILS ENGINEER
HUNTER

Title: OVERALL SITE PLAN

Project Number: 21074
Drawn by: AW
Date: 04/11/25
Revision:

Sheet:

1-DAB-A1.1



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CITY OF RIVERSIDE, CA

Consultants:

CIVIL	-
STRUCTURAL	-
MECHANICAL	-
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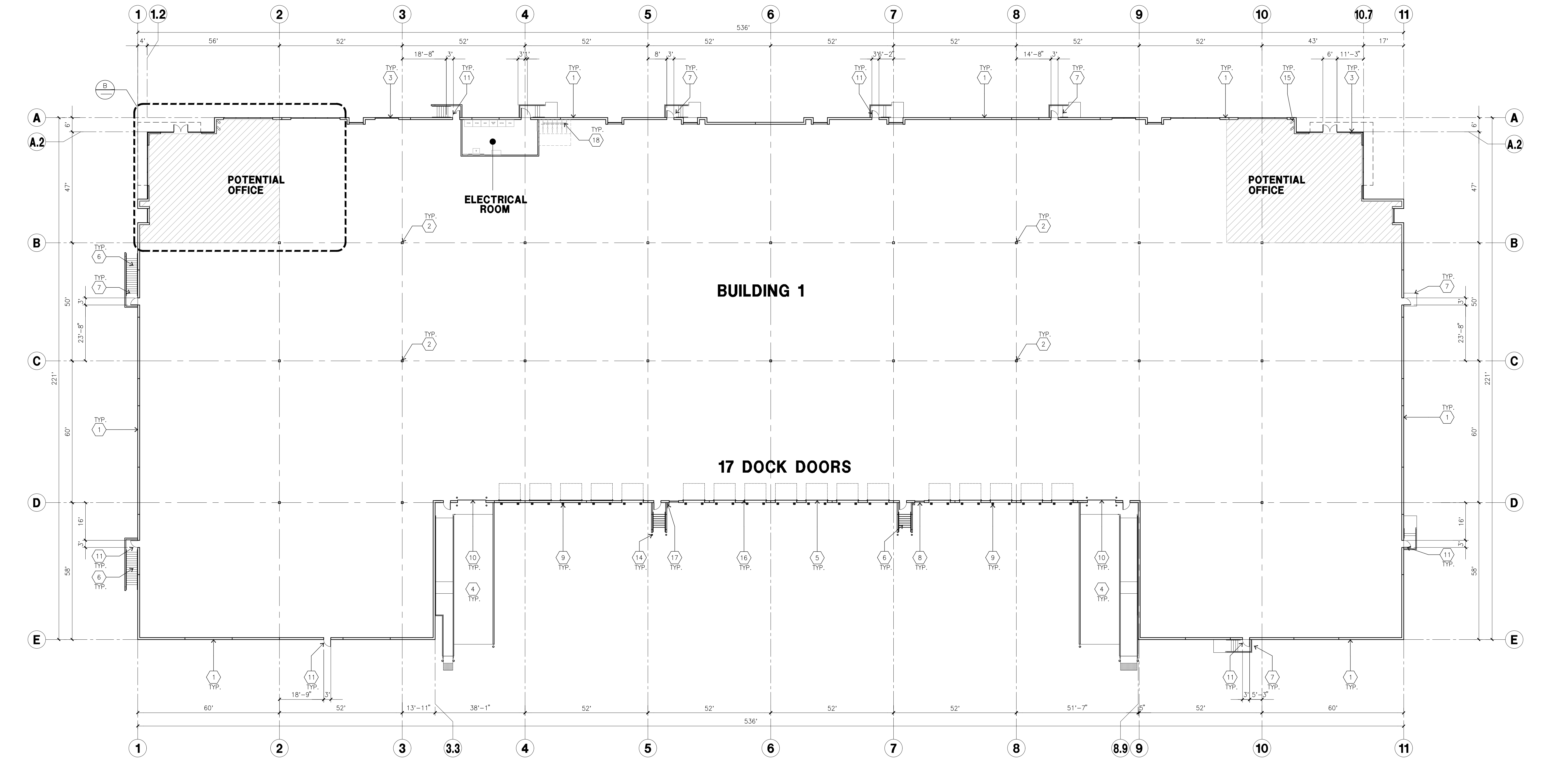
HUNTER

Title: OVERALL FLOOR PLAN

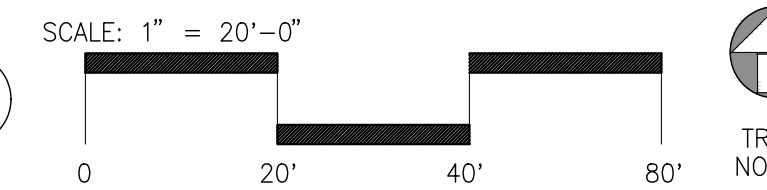
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Drawn by: AW
Date: 03/28/25
Revision:

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OVERALL FLOOR PLAN A



KEYNOTES - FLOOR PLAN

- CONCRETE TILT-UP PANEL.
- STRUCTURAL STEEL COLUMN.
- TYPICAL STOREFRONT SYSTEM WITH GLAZING. SEE OFFICE BLOW-UP AND ELEVATIONS FOR SIZE, COLOR AND LOCATIONS.
- CONCRETE RAMP W/ 42" HIGH CONC TILT-UP GUARD WALL BLOW-UP AND WALL ON BOTH SIDE OF RAMP.
- 9'-0" X 10' TRUCK DOOR, SECTIONAL O.H., STANDARD GRADE.
- EXTERIOR CONCRETE STAIR
- 5'-6"x5'-6"x4" THICK CONCRETE EXTERIOR LANDING PAD TYPICAL AT ALL EXTERIOR MAN DOORS TO LANDSCAPED AREA. FINISH TO BE MEDIUM BLOW FINISH. SLOPE TO BE 1/4" - 1/2" MAX. PROVIDE WALK TO HARD SURFACE PER CITY REQUIREMENTS.
- LOUVERED OPENING FOR VENTILATION.
- DOCK DOOR BUMPER
- 12' X 14' DRIVE THRU. SECTIONAL OH., STANDARD GRADE.
- 3'X7' HOLLOW METAL EXTERIOR MAN DOOR.
- SOFFIT LINE ABOVE
- ROOF HATCH ACCESS LADDER
- CONC. FILLED GUARD POST. 6" DIA. U.N.O.. 42"H
- INTERIOR ROOF DRAIN WITH OVERFLOW SCUPPERS
- Z GUARD FOR ALL OVERHEAD DOORS
- EXTERIOR DOWNSPOUT WITH OVERFLOW SCUPPERS
- 6-SPACE BIKE RACK

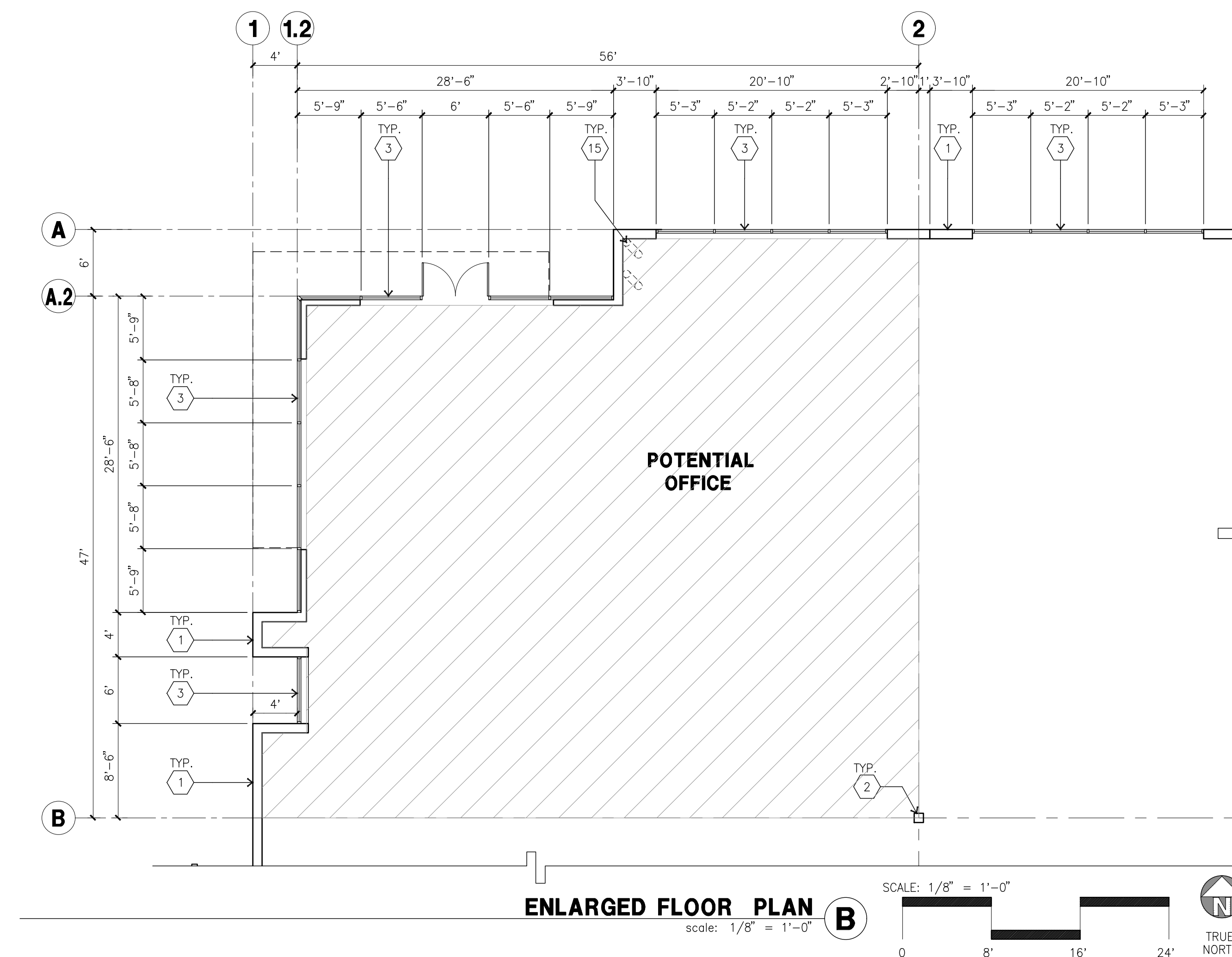
GENERAL NOTES-FLOOR PLAN

- A. THIS BUILDING IS DESIGNED FOR HIGH PILE STORAGE WITH FIRE ACCESS MAN DOORS AT 100' MAXIMUM O.C. A SEPARATE PERMIT WILL BE REQUIRED FOR ANY RACKING/CONVEYER SYSTEMS.
- B. FIRE HOSE LOCATIONS SHALL BE APPROVED PER FIRE DEPARTMENT
- C. THE BUILDING FLOOR SLAB IS SLOPED, SEE "C" DRAWINGS FOR FINISH SURFACE ELEVATIONS.
- D. NOT USED
- E. WAREHOUSE INTERIOR CONCRETE WALLS ARE PAINTED WHITE. COLUMNS ARE TO RECEIVE PRIMER ONLY. ALL GYP. BD. WALLS IN WAREHOUSE TO RECEIVE 1 COAT OF WHITE TO COVER.
- F. SLOPE POUR STRIP 1/2" TO EXTERIOR AT ALL MANDOR EXITS.
- G. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE PANEL WALL, GRIDLINE, OR FACE OF STUD U.N.O.
- H. SEE CIVIL DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR TO VERIFY ACTUAL UTILITY LOCATIONS. PLUMBING/ELECTRICAL COORDINATION.
- I. FOR DOOR TYPES AND SIZES, SEE DETAIL SHEET AD.4. NOTE: ALL DOORS PER DOOR SCHEDULE ARE FINISH OPENINGS.
- J. CONTRACTOR TO PROTECT AND KEEP THE FLOOR SLAB CLEAN. ALL EQUIPMENT TO BE DIAPERED INCLUDING CARS AND TRUCKS.
- K. ALL EXIT MAN DOORS IN WAREHOUSE TO HAVE ILLUMINATED EXIT SIGN.
- L. HIGHLY FLAMMABLE AND COMBUSTIBLE MATERIAL SHALL NOT BE USED OR STORED IN THIS BUILDING.
- M. EACH EXTERIOR EXIT DOOR SHALL BE IDENTIFIED BY A TACTILE EXIT SIGN WITH THE WORDS "EXIT". THE MOUNTING HEIGHT FOR SUCH SIGNAGE SHALL BE 60" FROM FINISH FLOOR LEVEL TO THE CENTER OF THE SIGN.
- N. NON-ACCESSIBLE DOOR. PROVIDE WARNING SIGN LOCATED IN THE INTERIOR SIDE PER CBC 1133B.1.1.1
- O. ALL ROOF MOUNTED MATERIALS SHALL BE FULLY SCREENED FROM PUBLIC VIEW, SEE A/J44.1 OFFICE SECTION.

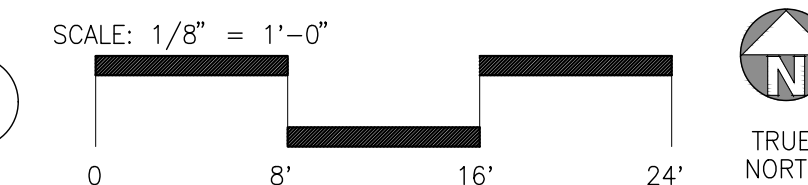
FLOOR SLAB AND POUR STRIPS REQ.

THESE NOTES ARE VERY MIN. REQUIREMENT.

- FLOOR COMPACTION - 95%
- TRENCH COMPACTION - 90%
- BUILDING FLOOR SLAB:
- CONTRACTOR TO BUILD FOR CLASS V FLOOR PER A.C.I. 302-IR-96
- NOT USED
- CONCRETE SLAB TO HAVE STEEL FLOAT HARD TROWEL BURNISHED FINISH.
- FOR SLAB CURING, COORDINATE WITH THE OWNER.
- ALL EQUIPMENT & MOVING VEHICLES SHALL BE DIAPERED.
- NO CRANES, CONCRETE TRUCKS, OR ANYTHING HEAVIER WILL BE PLACED ON THE SLAB.
- SLAB TO BE FF50 FL35 MEASURED WITHIN 24 HOURS.
- NO FLY ASH IN THE CONCRETE
- WHERE  INDICATED, PROVIDE VAPOR BARRIER, CONCRETE SLAB OVER 2" SAND OVER 10MIL VISQUEEN, OVER 2" SAND, OVER COMPACT SOIL. ENGINEER OR MANUFACTURER'S RECOMMENDATION.
- CONCRETE SLAB IN FUTURE OFFICE AREAS, WHERE SAND OVER VISQUEEN ARE REQUIRED, SHALL BE NATURALLY HYDRATED WITHOUT USE OF BURLENE, CURING COMPOUND, OR RELEASE AGENTS. CONTROL/CONSTRUCTION JOINTS SHALL NOT BE FILLED WITH MM-80 JOINT FILLER IN FUTURE OFFICE AREAS.
- SEAL CONCRETE SLAB W/ "LAPIDOLITH" SEALER

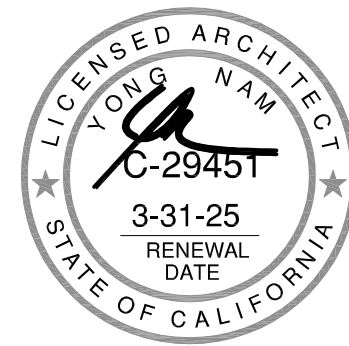


ENLARGED FLOOR PLAN B





HPA, INC.
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email: hpa@hparchs.com



Owner:



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TEL: 310-929-8088

Project:

MASSACHUSETTS AVE.
& KANSAS AVE. BY
STALEY POINT

CITY OF RIVERSIDE, CA

Consultants:

CIVIL -
STRUCTURAL -
MECHANICAL -
PLUMBING -
ELECTRICAL -
LANDSCAPE HUNTER -
FIRE PROTECTION -
SOILS ENGINEER -

Title: OVERALL ROOF PLAN

Project Number: 21074

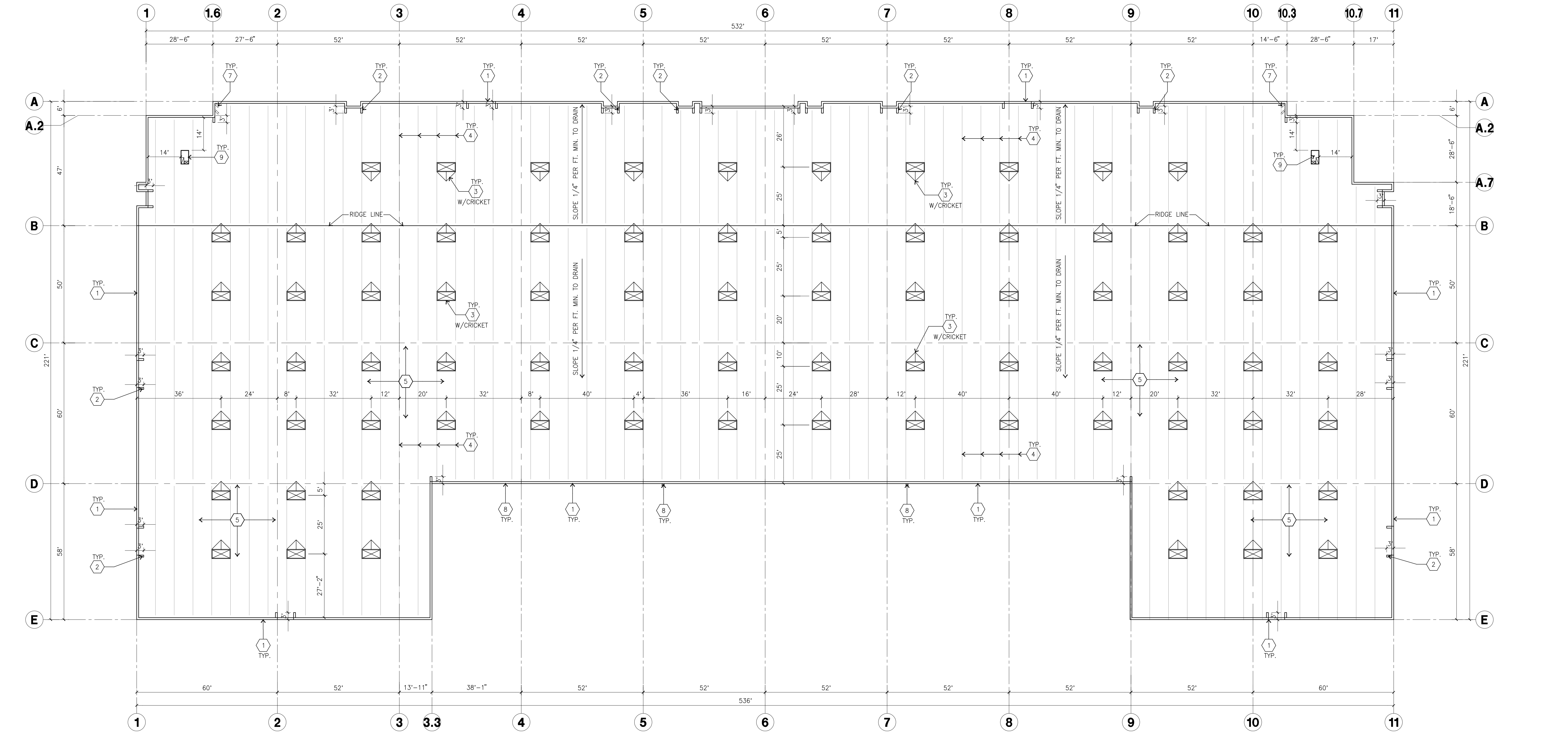
Drawn by: AW

Date: 08/01/24

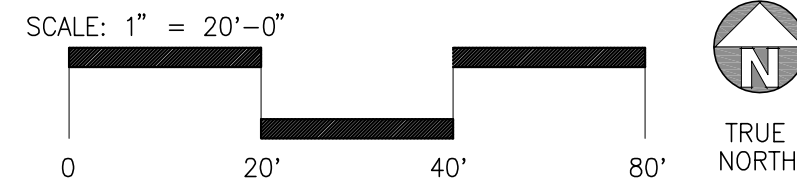
Revision:

Sheet:

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OVERALL ROOF PLAN
scale: 1" = 20'-0" A



ROOF PLAN KEYNOTES

- 1 CONCRETE PARAPET. SEE "S" DRAWINGS FOR THICKNESS
- 2 PARAPET RETURN
- 3 4"x8" CURB MOUNTED SKYLIGHT.
- 4 STRUCTURAL JOIST. SEE "S" DRAWINGS.
- 5 PROVIDE CERTAINTED 4-PLY BUILT-UP ROOFING, OVER ROOF SHEATHING OVER ROOF FRAMING. U.N.O. 10 YEAR NO DOLLAR LIMIT WARRANTY AND 2 YEAR LABOR GUARANTEE
- 6 INTERIOR ROOF DRAIN WITH OVERFLOW SCUPPERS.
- 7 INTERIOR ROOF DRAIN WITH OVERFLOW INTERIOR DRAIN.
- 8 EXTERIOR DOWNSPOUT WITH OVERFLOW SCUPPERS
- 9 FUTURE ROOF TOP UNIT

ROOF PLAN GENERAL NOTES

- A. ALL DIMENSIONS ARE TO FACE OF CONCRETE WALL, GRIDLINE, OR FACE OF STUD. (U.N.O.)
- B. VERIFY THAT ALL ROOF AREAS HAVE POSITIVE DRAINAGE(1/4" FT.) PRIOR TO ROOF INSULATION INSTALLATION.
- C. REFER TO "S" DRAWINGS FOR ROOF HEIGHT ELEVATIONS IN REGARDS TO ROOF DRAINAGE.
- D. CONTRACTOR TO VERIFY AND COORDINATE WITH ALL SUBCONTRACTORS, ALL LOCATIONS AND SIZES OF ROOF OPENINGS.
- E. FOR TYP. ROOF PENETRATIONS, SEE DETAIL 6, 9, 10/AD.3 CONTRACTOR TO VERIFY ALL LOCATIONS WITH MECHANICAL AND PLUMBING DRAWINGS
- F. CONTRACTOR TO VERIFY ALL ROOF DRAIN DEPRESSIONS W/ "S" DRAWINGS.
- G. CONTRACTOR TO COORDINATE EXACT SKYLIGHT LOCATION W/ FIRE SPRINKLER AND ROOF FRAMING SUBCONTRACTORS. LOCATIONS ON DRAWINGS ARE APPROXIMATE.
- H. ALL SKYLIGHTS TO BE DESIGNED PER WIND SPEED DESIGN CRITERIA W/ STRONGER FRAMES, SUPPORTS AND DOMES. MANUFACTURER TO CONFIRM IN WRITING.
- I. ROOF DRAINS AND OVERFLOW DRAINS TO BE A MIN. 8" DIA., SEE "P" DRAWINGS FOR EXACT SIZE.
- J. PROVIDE SHAPED CRICKETS AS REQUIRED FOR PROPER ROOF DRAINAGE OF 1/4" PER FOOT (MIN.).
- K. BUILT UP ROOFING TO BE DESIGNED FOR I 90 AND TO BE A U.L. CLASS A ROOF. FLASHING: GAF TYPE 5MB MOPPED AP SHEET, U.L. TYPE 63 BUR.
- L. ALL ROOFING FASTENERS SHALL MEET ALL CODE AND REGULATION REQUIREMENTS. PROPER REQUIREMENTS ARE IN PROJECTS JURISDICTION AND ALSO, FACTORY MUTUAL (FM) WIND UPLIFT PRESSURE REQUIREMENTS.
- M. ALL SKYLIGHTS TO BE PLACED A MIN. OF 18"-0" FROM INSIDE FACE OF PARAPET
- N. USE WHITE SCRIM FOIL INSULATION IN THE WAREHOUSE AREA.
- O. SMOKE VENTS SHALL BE REQUIRED IF AREAS FOR "POTENTIAL OFFICES" WILL BE USED FOR HIGH PILE COMBUSTIBLE STORAGE.
- P. ROOF DRAINS, OVERFLOW DRAINS AND RAINWATER PIPING WITHIN THE INTERIOR OF THE BUILDING SHALL BE TESTED IN ACCORDANCE WITH THE PROVISIONS OF THE PLUMBING CODE FOR TESTING DRAIN, WASTE AND VENT SYSTEMS
- Q. ROOF DRAINS, OVERFLOW PIPING WITHIN THE BUILDING SHALL UTILIZE APPROVED DRAINAGE FITTINGS.
- R. MANUFACTURED ROOF DRAINS AND OVERFLOWS(FIXTURES) SHALL BE IAPMO LISTED
- S. CERTAINTED BUR OR SBS BASE SHEET MECHANICALLY ATTACHED TO APPROVED SUBSTRATE, APPROVED INTERPLY SHEETS SET IN HOT ASPHALT, APPROVED MINERAL-SURFACED CAP SHEET SET IN HOT ASPHALT. ASPHALT TYPE II OR TYPE IV. IN ANGLES OF ROOF DECK AND VERTICAL SURFACES, THE ROOFING CONTRACTOR SHALL FURNISH AND INSTALL AN APPROVED CANT STRIP WITH A MIN. 3" FACE. ROOF MAX. SLOPE 6":12". CLASS A RATED. ICC-ES EVALUATION REPORT NO. ESR-1388.
- SUMMARY OF ROOFING MATERIALS:
- BASE SHEET (1 PLY)
 - INTERPLY SHEETS (2 PLY)
 - CAP SHEET (1 PLY)
 - ASTM D 312 ASPHALT (THREE MOPPINGS)
- T. SEE ACOUSTIC REPORT FOR ROOF-CEILING ASSEMBLY REQUIREMENT FOR STC RATING

ROOF LEGEND

- | | | | |
|--|---------------------------------------|--|---|
| | 4' X 8' SKYLIGHT
SEE DETAIL 5/AD.3 | | BUILDING PARAPET LINE |
| | ROOF HATCH | | EXTERIOR DOWNSPOUT
W/ OVERFLOW SCUPPER |
| | FUTURE ROOF TOP UNIT | | INTERIOR ROOF DRAIN
W/ OVERFLOW SCUPPERS |

SKYLIGHT CALCULATION

PROJECT:
CLEAR HEIGHT = 32 FT. CLEAR

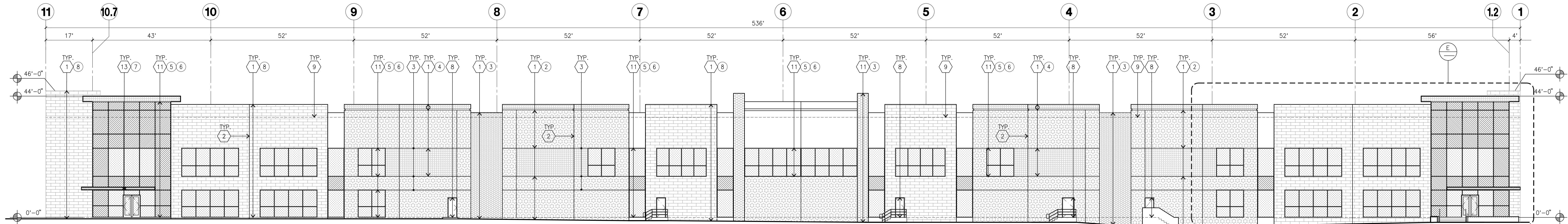
REQUIRED:
BUILDING FOOTPRINT = 99, 900 S.F. / 93, 900 S.F. WITHOUT OFFICE
SKYLIGHT REQUIRED = VOLUME OF BUILDING /9000 WITH AUTOMATIC SPRINKLER
VOLUME OF BUILDING = 3, 196 800 C.F.
3, 196 800 C.F. / 9,000 S.F.= 355.2 S.F.

SKYLIGHT REQUIRED = 2.5 % = 2,497.5 S.F. (78 SKYLIGHTS, 32 S.F. EA)

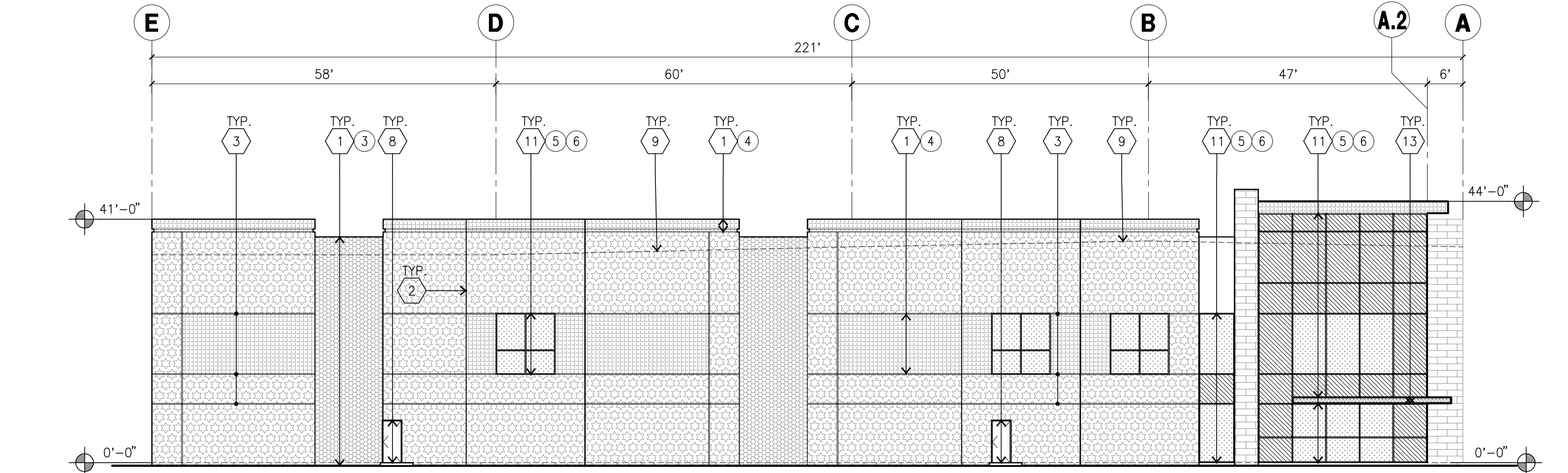
PROVIDED:
SKYLIGHT PROVIDED = 2.5 % = 2,497.5 S.F. (78 SKYLIGHTS, 32 S.F. EA)

MODEL:
SKYLIGHT MODEL = MANUFACTURED BY BRISTOLITE DAYLIGHTING SYSTEM
MODEL # T4896-SS-CM-A-HS1 OR EQUAL

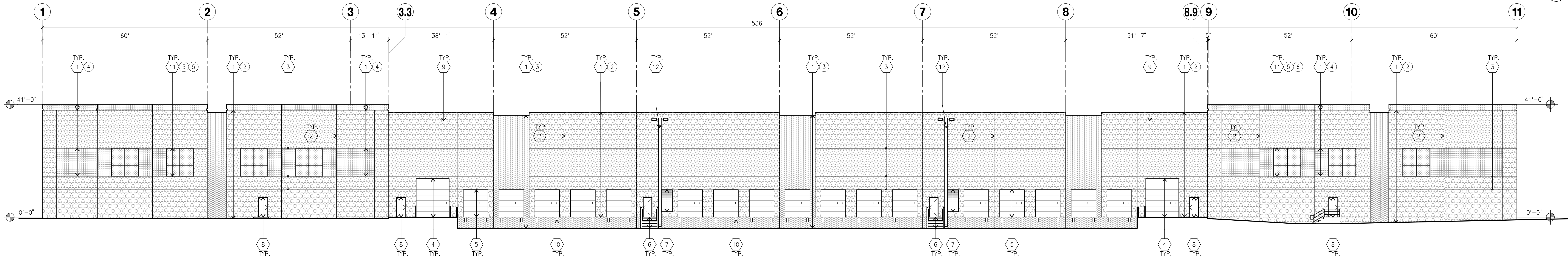
BULGAR BAR MODEL = PROVIDED BULGAR BAR AT ALL SKYLIGHT LOCATION-TYP.



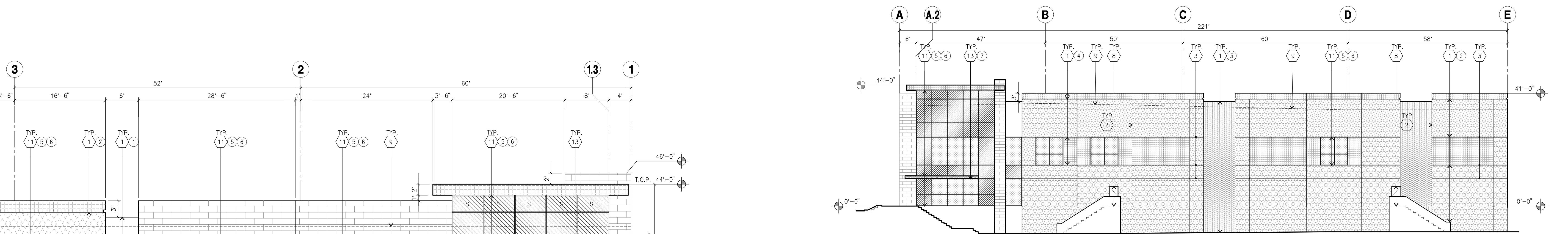
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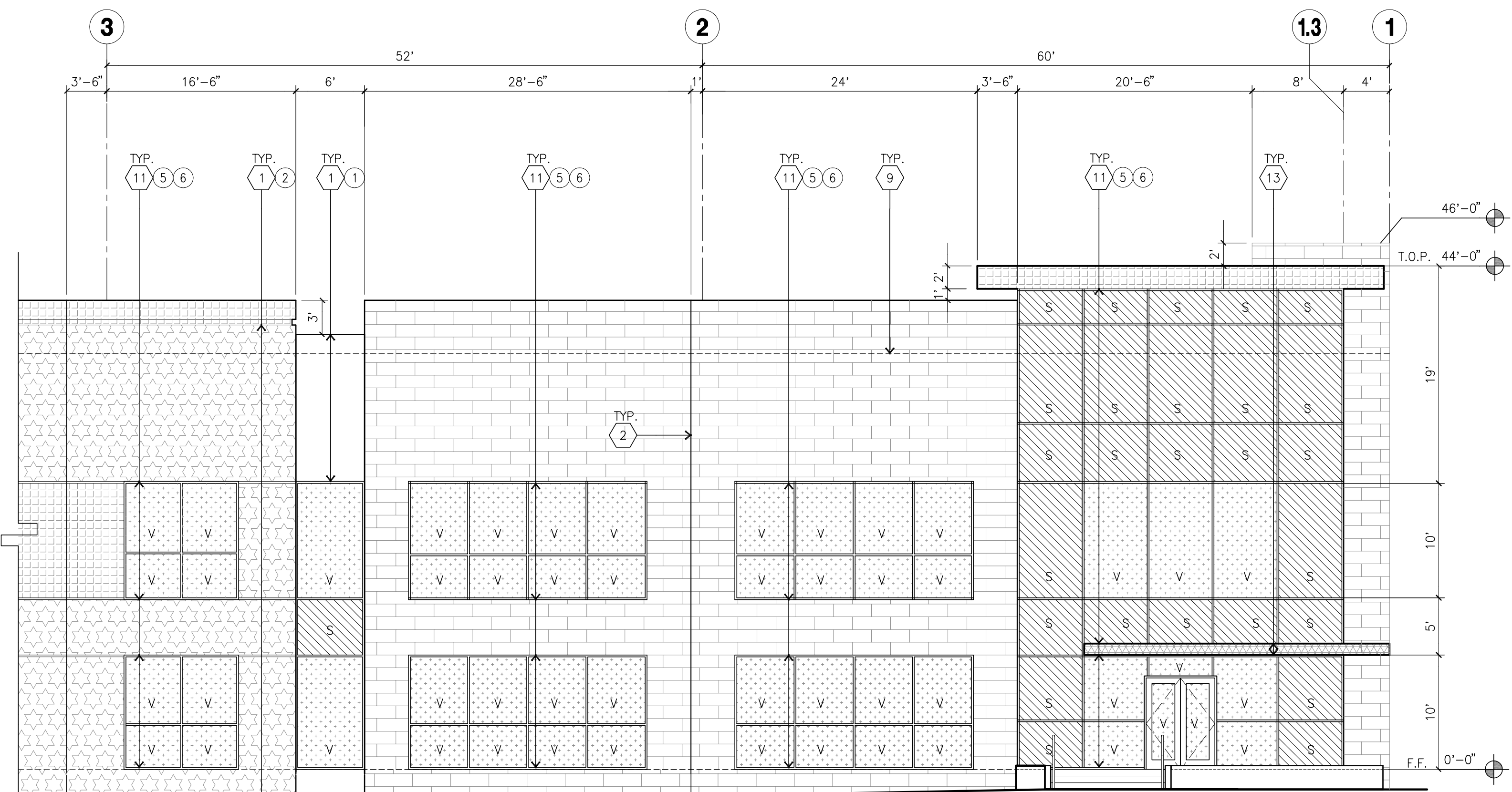
EAST ELEVATION
scale: 1/16" = 1'-0"



SOUTH ELEVATION
scale: 1/16" = 1'-0"



WEST ELEVATION
scale: 1/16" = 1'-0"



ENLARGED NORTH ELEVATION
scale: 1/8" = 1'-0"

KEYNOTES - ELEVATIONS

- CONCRETE TILT-UP PANEL(PAINTED). FINISH GRADE VARIES. SEE "C" DRAWINGS. WATERPROOF ALL WALLS WHERE GRADE IS HIGHER AND EXPOSED TO THE WEATHER ONE SIDE. WATERPROOFING TO BE PROTECTED WITH PROTECTION BOARD AND A MIN. OF 6" OF DRAIN. PROVIDE TRENCH DRAIN AT BOTTOM AND DAYLIGHT TO CURB OR TAKE TO STORM DRAIN. NOT REQUIRED AT DOCK HIGH CONDITION OR AT RAMP WALLS. PAINT COLOR TO MATCH FIELD COLOR.
 - PANEL JOINT.
 - PANEL REVEAL. ALL REVEALS TO HAVE A MAX. OF 3/8" CHAMFER. REVEAL COLOR TO MATCH ADJACENT BUILDING FIELD COLOR. U.N.O.
 - 12"x14" OVERHEAD DOOR @ DRIVE THRU. PROVIDE COMPLETE WEATHER-STRIPPING PROTECTION ALL AROUND. PAINT COLOR TO MATCH FIELD COLOR.
 - 8'x10' OVERHEAD DOOR @ DOCK HIGH. PROVIDE COMPLETE WEATHER-STRIPPING PROTECTION ALL AROUND. PAINT COLOR TO MATCH FIELD COLOR.
 - CONCRETE STAIR, LANDING AND GUARDRAIL W/ METAL PIPE HANDRAIL. PROVIDE NON SKID NOSING TO MEET ADA REQUIREMENTS. PROVIDE CONTRASTING COLORED 3" WIDE WARNING STRIPE INTEGRAL TO CONCRETE AT TOP LANDING AND BOTTOM TREAD PER ADA REQUIREMENTS.
 - METAL LOUVER. PAINT COLOR TO MATCH FIELD COLOR.
 - HOLLOW METAL DOORS. PROVIDE COMPLETE WEATHER STRIPPING ALL AROUND DOOR. PROVIDE FOR RAIN DIVERTER ABOVE DOOR.
 - ROOF LINE BEYOND.
 - DOCK BUMPER
 - ALUMINUM STOREFRONT FRAMING WITH TEMPERED GLAZING AT ALL DOORS, SIDEITES ADJACENT TO DOORS AND GLAZING
 - EXTERIOR DOWNSPOUT WITH OVERFLOW SCUPPERS
 - EXTERIOR CANOPY
 - INTERIOR ROOF DRAIN WITH OVERFLOW SCUPPERS
- GENERAL NOTES - ELEVATIONS**
- A. ALL PAINT COLOR CHANGES TO OCCUR AT INSIDE CORNERS UNLESS NOTED OTHERWISE.
B. ALL PAINT FINISHES ARE TO BE FLAT UNLESS NOTED OTHERWISE.
C. T.O.P. FL= TOP OF PARAPET ELEVATION.
D. F.F. = FINISH FLOOR ELEVATION.
E. STOREFRONT CONSTRUCTION: GLASS, METAL ATTACHMENTS AND UNITS. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS PRIOR TO INSTALLATION.
F. CONTRACTOR SHALL FULLY PAINT ONE CONCRETE PANEL W/ SELECTED COLORS. ARCHITECT AND OWNER SHALL APPROVE PRIOR TO PAINTING REMAINDER OF BUILDING.
G. BACK SIDE OF PARAPETS TO HAVE SMOOTH FINISH AND BE PAINTED WITH ELASTOMERIC PAINT.
H. FOR SPANDREL GLAZING, ALLOW SPACE BEHIND SPANDREL TO BREATHE.
I. USE ADHESIVE BACK WOOD STRIPS FOR ALL REVEAL FORMS.
K. THE FIRST COAT OF PAINT TO BE ROLLED-ON AND THE SECOND COAT TO BE SPRAYED-ON.
P. EXTERIOR WALL SHALL BE PROVIDED WITH A GRAFFITI-RESISTANT COATING OR PAINT, TO A HEIGHT OF 12' FOR BUILDING OR STRUCTURE SURFACES THAT ARE NOT INTENDED TO BE PAINTED (E.G. SPLIT-FACE BLOCK, DECORATIVE TILE, ETC.).

COLOR SCHED. - ELEVATIONS

- | | |
|---|--|
| 1 | CONCRETE TILT-UP PANEL PAINT BRAND SHERWIN-WILLIAMS SW7005 PURE WHITE |
| 2 | CONCRETE TILT-UP PANEL PAINT BRAND SHERWIN-WILLIAMS SW7043 WORLDLY GRAY |
| 3 | CONCRETE TILT-UP PANEL PAINT BRAND SHERWIN-WILLIAMS SW7045 INTELLECTUAL GRAY |
| 4 | CONCRETE TILT-UP PANEL PAINT BRAND SHERWIN-WILLIAMS SW6524 COMMODORE |
| 5 | MULLIONS |
| 6 | GLAZING |
| 7 | METAL CANOPY |
| 8 | CORONADO STONE |
- COLOR: SHERWIN-WILLIAMS SW7076 CYBERSPACE
COLOR: BLUE REFLECTIVE GLASS
SHERWIN-WILLIAMS ACRYLIC LATEX SYSTEM HIGH
PAINT BRAND GLOSS/HIGH PERFORMANCE SW6524 COMMODORE
STYLE: WIRECUT BRICK
COLOR: LA JOLLA BLEND
SIZE: 2 1/2" X 8"

GLAZING LEGEND

- | | |
|--|---------------------------------------|
| TEMPERED SPANDREL GLASS | TEMPERED VISION INSULATED GLASS |
| PRG: VISTACOL (2) PACIFICA + SOLARBAN | SOLARBAN 60 (3) CLEAR |
| U VALUE: 0.27, SHGC: 0.21 AND VLT: 26% | 1" INSULATED GLASS WITH 1/2" AIRSPACE |
| | AND (2) 1/4" LITES. |



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& KANSAS AVE. BY
STALEY POINT

CITY OF RIVERSIDE, CA

Consultants:

CIVIL -
STRUCTURAL -
MECHANICAL -
PLUMBING -
ELECTRICAL -
LANDSCAPE -
FIRE PROTECTION -
SOILS ENGINEER -
HUNTER

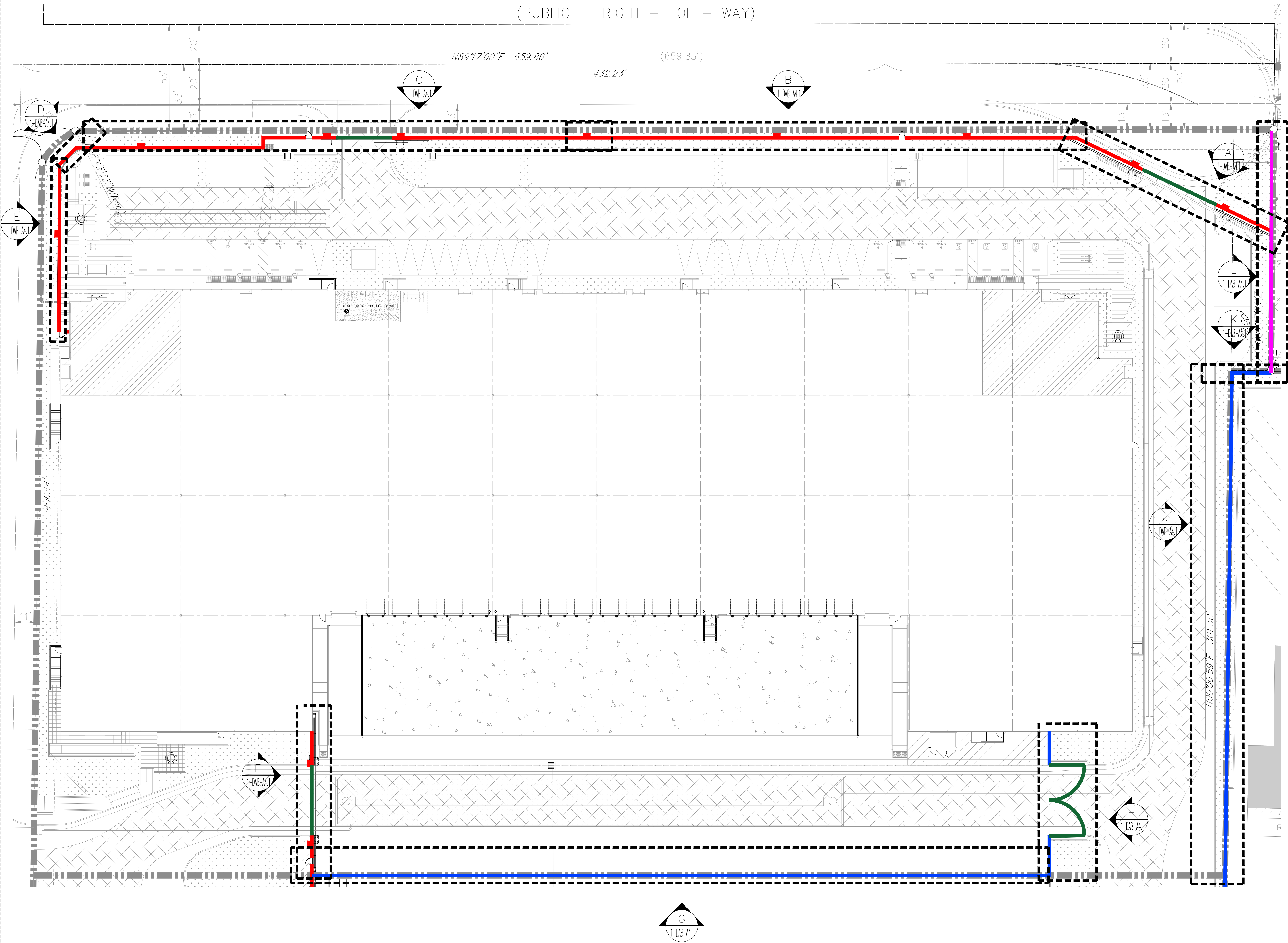
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Project Number: 21074
Drawn by: AW
Date: 08/01/24
Revision:

Sheet:

1-DAB-A3.1

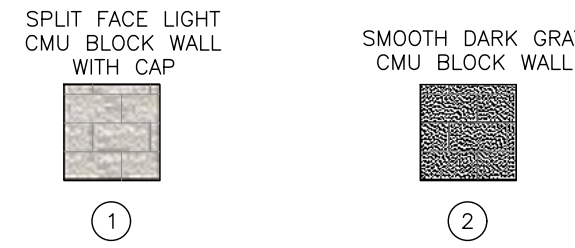
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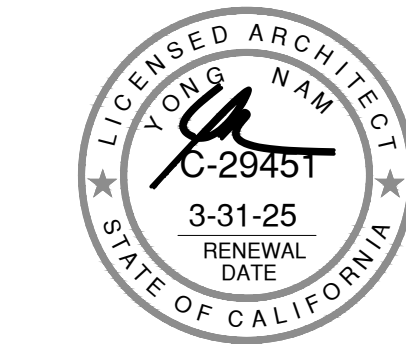
KEY NOTES

- 1 A) 4' CMU BLOCK WALL WITH CAP
B) 6' CMU BLOCK WALL WITH CAP (PILASTER)
C) PAINTED BLACK 2\"/>
- 2 PAINTED BLACK 6\"/>
- 3 PAINTED BLACK 6\"/>
- 4 6' CMU BLOCK

COLOR LEGEND



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ELECTRICAL
LANDSCAPE
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HUNTER

Title: KEYPLAN SCREENWALL

Project Number: 21074
Drawn by: AW
Date: 04/11/25
Revision:

Sheet:

1-DAB-A4.0



Project:

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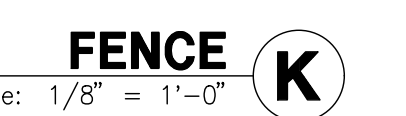
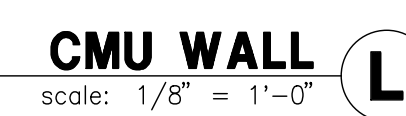
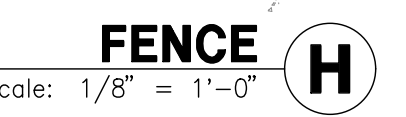
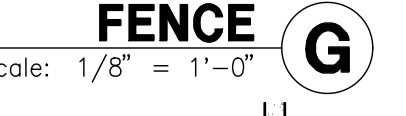
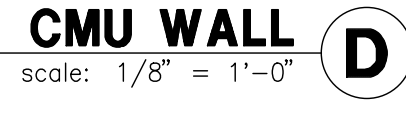
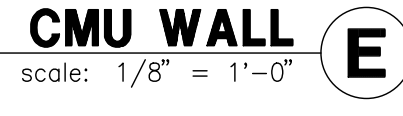
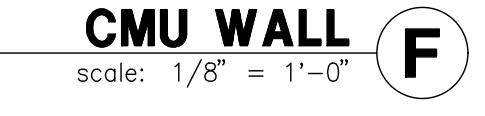
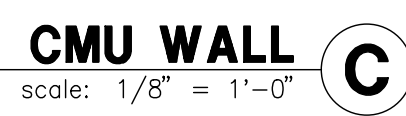
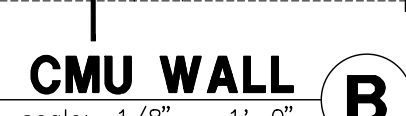
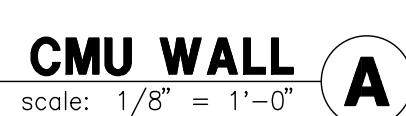


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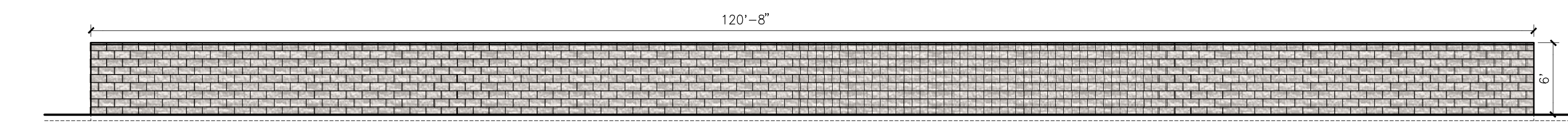
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STRUCTURAL	-
Mechanical	-
PLUMBING	-
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LANDSCAPE	HUNTER
ENVIRONMENTAL PROTECTION	-
SOILS ENGINEER	-

Revision:

1-DAB-A4.1

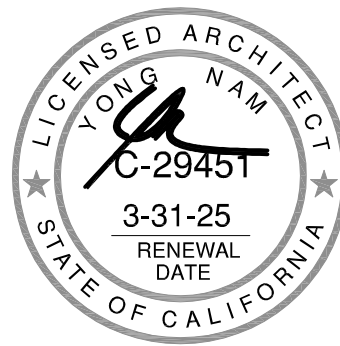
[illegible]

	1	A) 4'H CMU BLOCK WALL WITH CAP B) 6'H CMU BLOCK WALL WITH CAP (PILASTER) C) PAINTED BLACK 2'H TUBULAR STEEL FENCE
	2	PAINTED BLACK 6'H METAL SLIDING GATES
	3	PAINTED BLACK 6'H TUBULAR STEEL FENCE
	4	6'H CMU BLOCK





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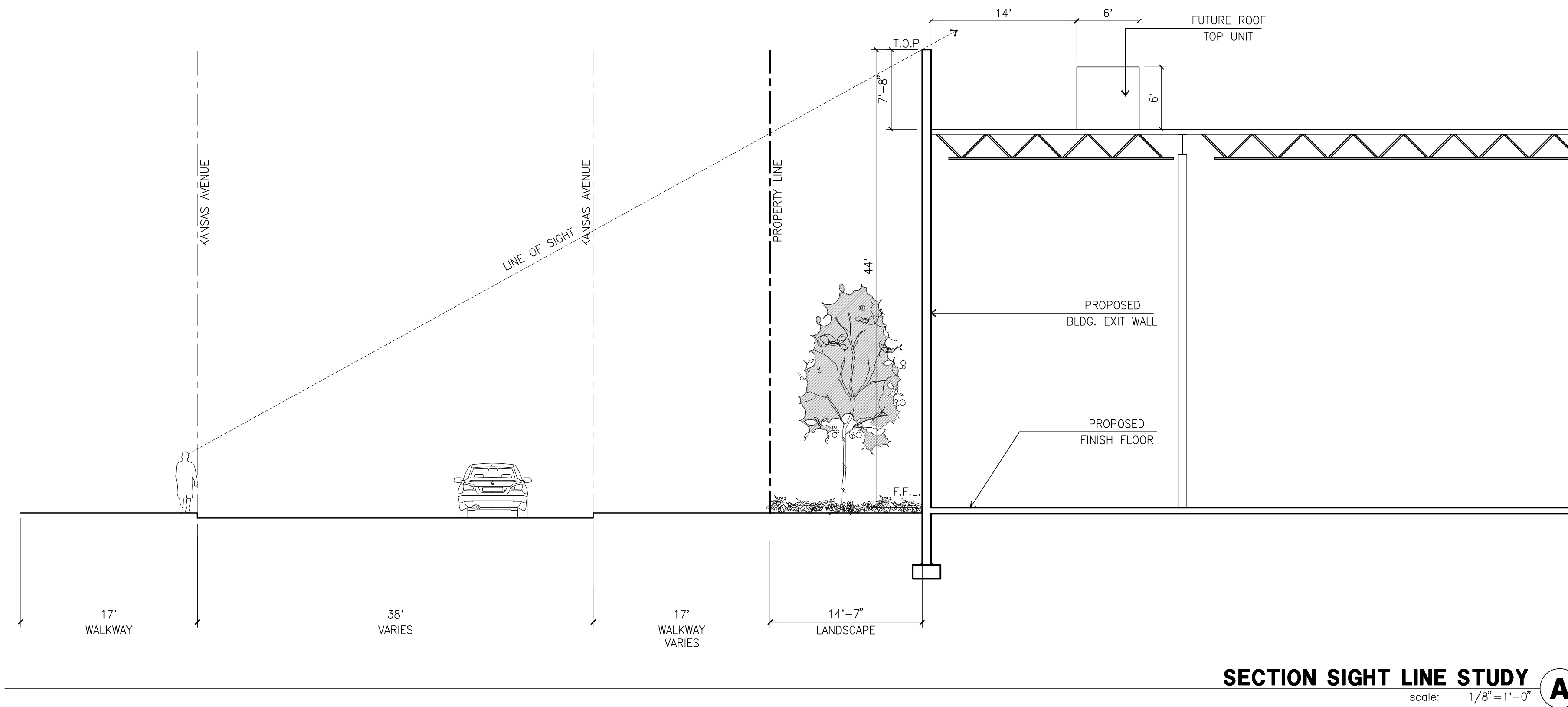
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| MECHANICAL | - |
| PLUMBING | - |
| ELECTRICAL | - |
| LANDSCAPE | HUNTER |
| FIRE PROTECTION | - |
| SOILS ENGINEER | - |

Title: SECTION SIGHT LINE STUDY

Project Number: 21074
Drawn by: AW
Date: 08/01/24
Revision:

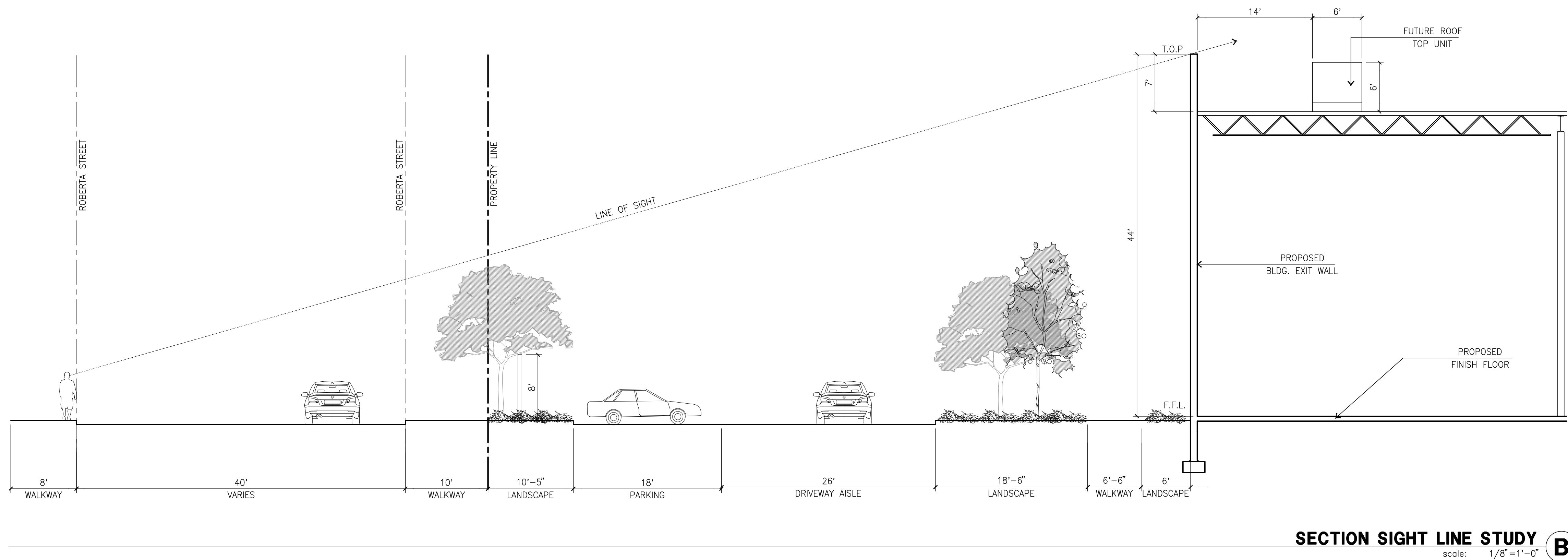
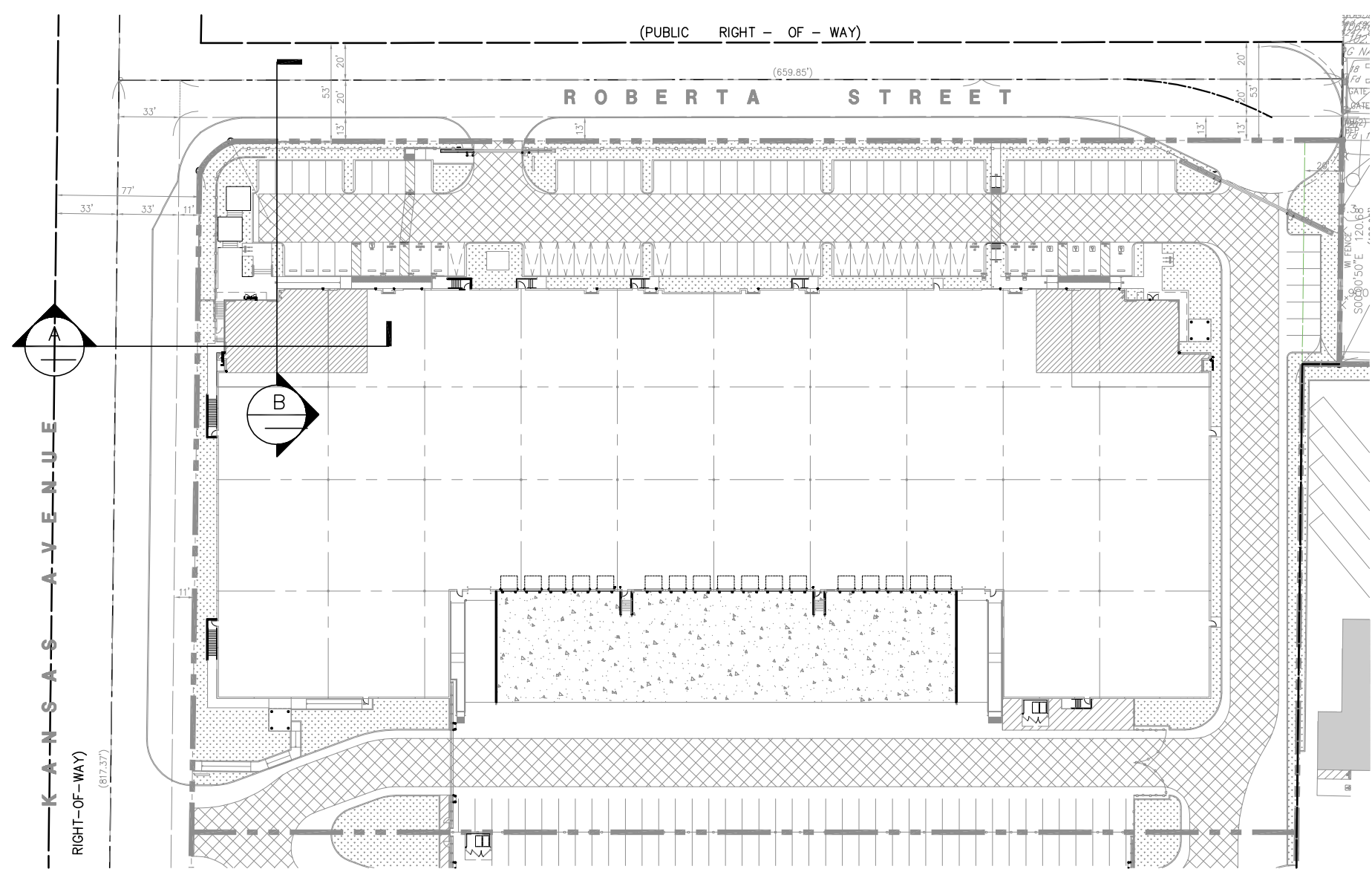
Sheet:

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SECTION SIGHT LINE STUDY A
scale: 1/8"=1'-0"

KEYPLAN



SECTION SIGHT LINE STUDY B
scale: 1/8"=1'-0"